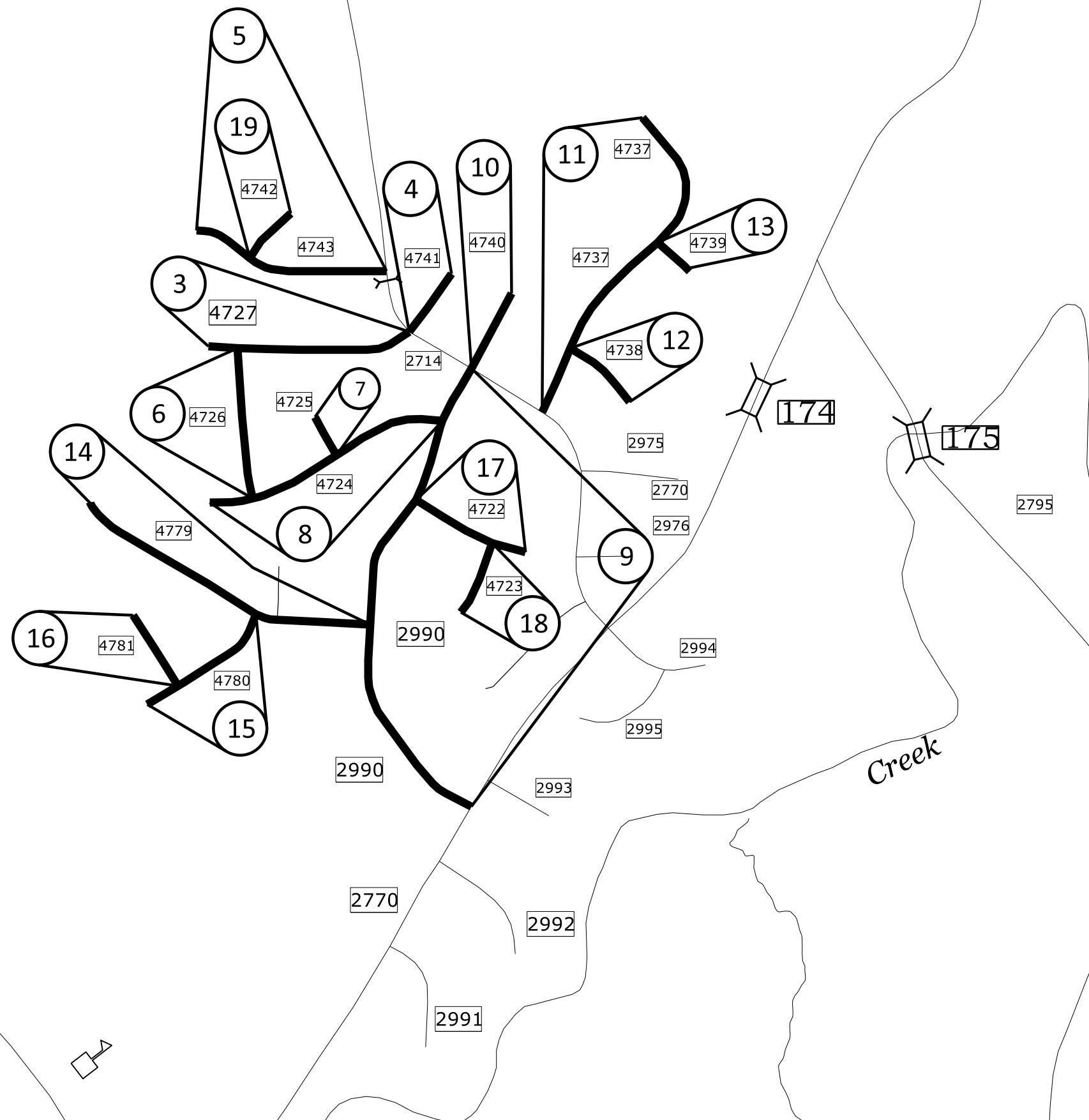
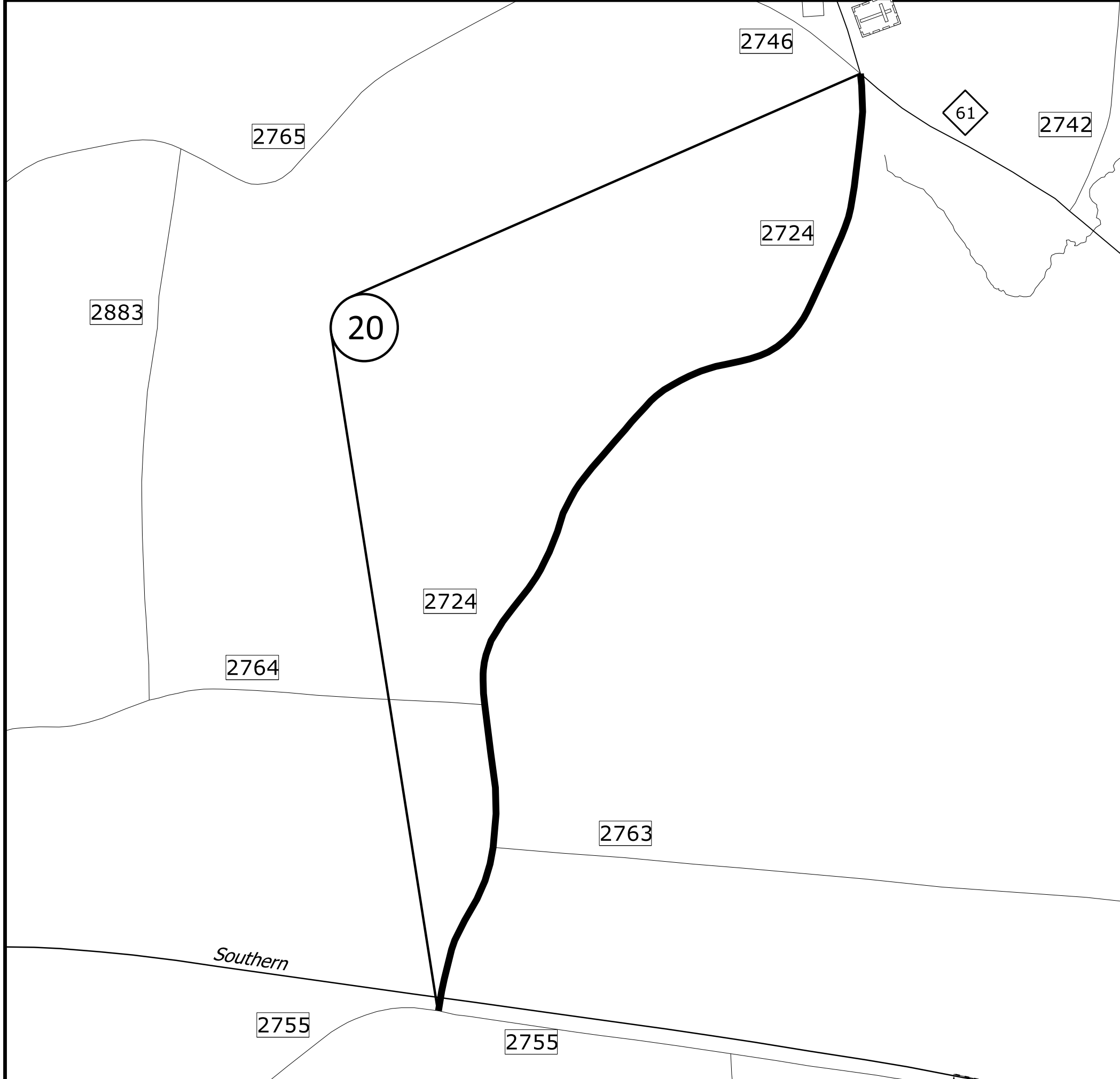


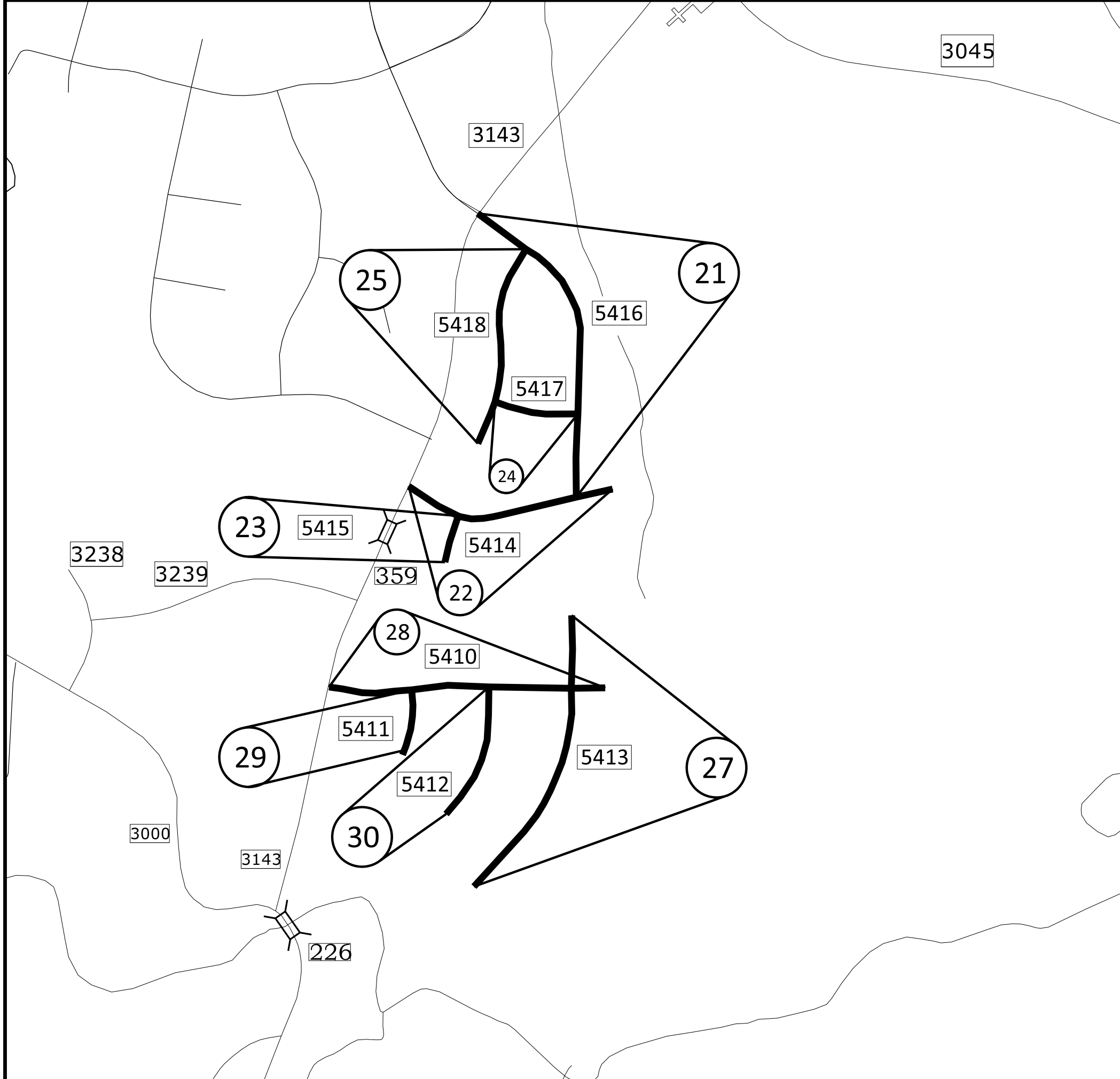
- Map 1 SR 2376 - Scottsdale Rd
Begin Project at end of curb and gutter.
- Map 2 SR 2632 - Larkhill Ct



- Map 3 SR 4727 - Begian Dr
- Map 4 SR 4741 - Belgian Ct
- Map 5 SR 4743 - Iron Weed Dr
- Map 6 SR 4726 - Stetson Dr
- Map 7 SR 4725 - Loreda Ct
- Map 8 SR 4724 - Quarter Horse Tr
- Map 9 SR 2990 - Saddlebranch Dr
- Map 10 SR 4740 - Saddlebranch Ct
- Map 11 SR 4737 - Benttree Dr
- Map 12 SR 4738 - Bridletree Ct
- Map 13 SR 4739 - Branding Ct
- Map 14 SR 4779 - Hitching Post Dr
- Map 15 SR 4780 - Pony Run Dr
- Map 16 SR 4781 - Pony Run Ct
- Map 17 SR 4722 - Saddler Tr
- Map 18 SR 4723 - Saddler Ct
- Map 19 SR 4742 - Chestnut Bay Ct



Map 20 SR 2724 - Wagoner Rd

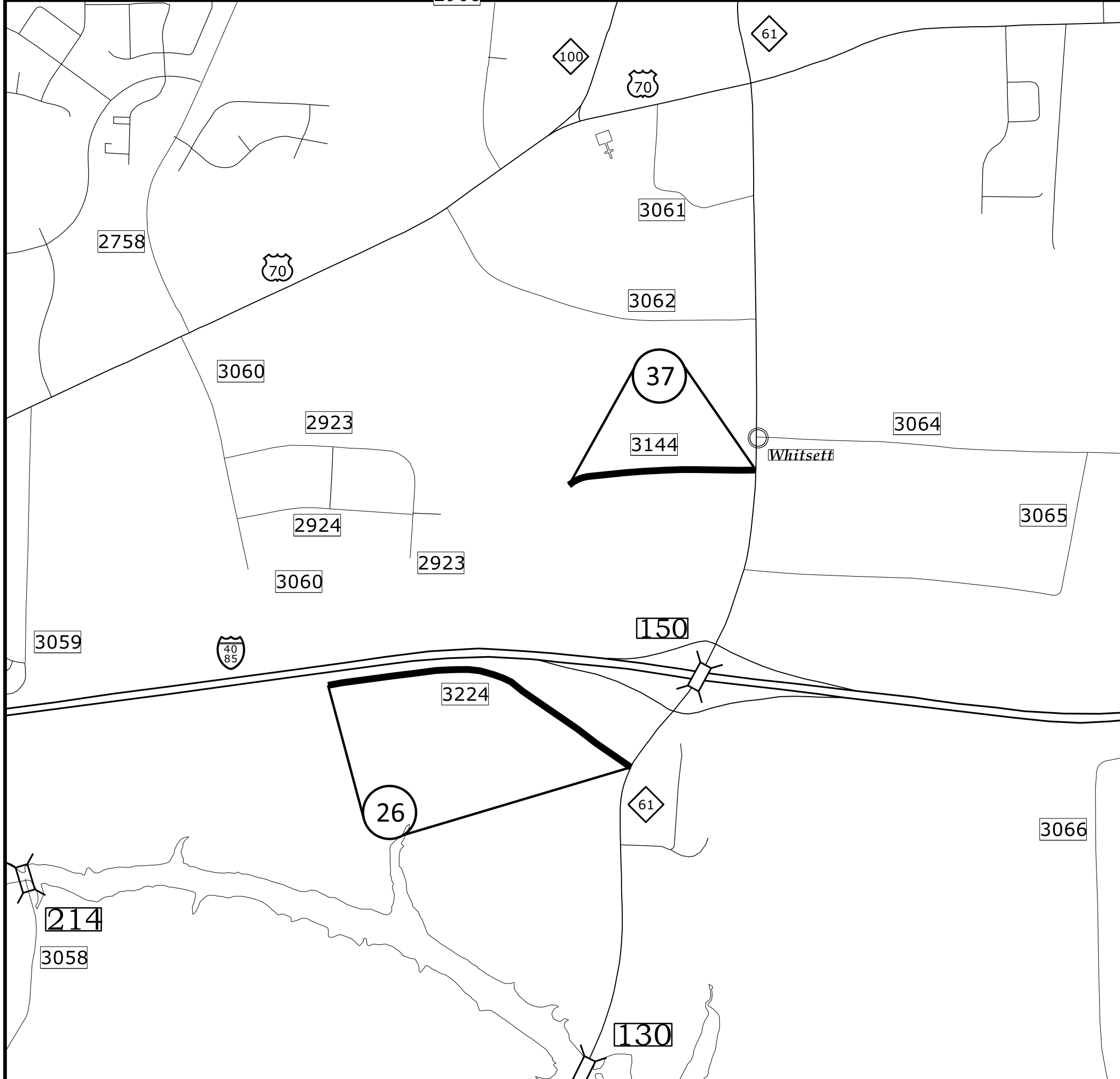


- Map 21 SR 5416 - Covered Wagon Rd
- Map 22 SR 5414 - Cragganmore Dr
- Map 23 SR 5415 - Oban Ct
- Map 24 SR 5417 - Laggan Dr
- Map 25 SR 5418 - Bowmore Pl

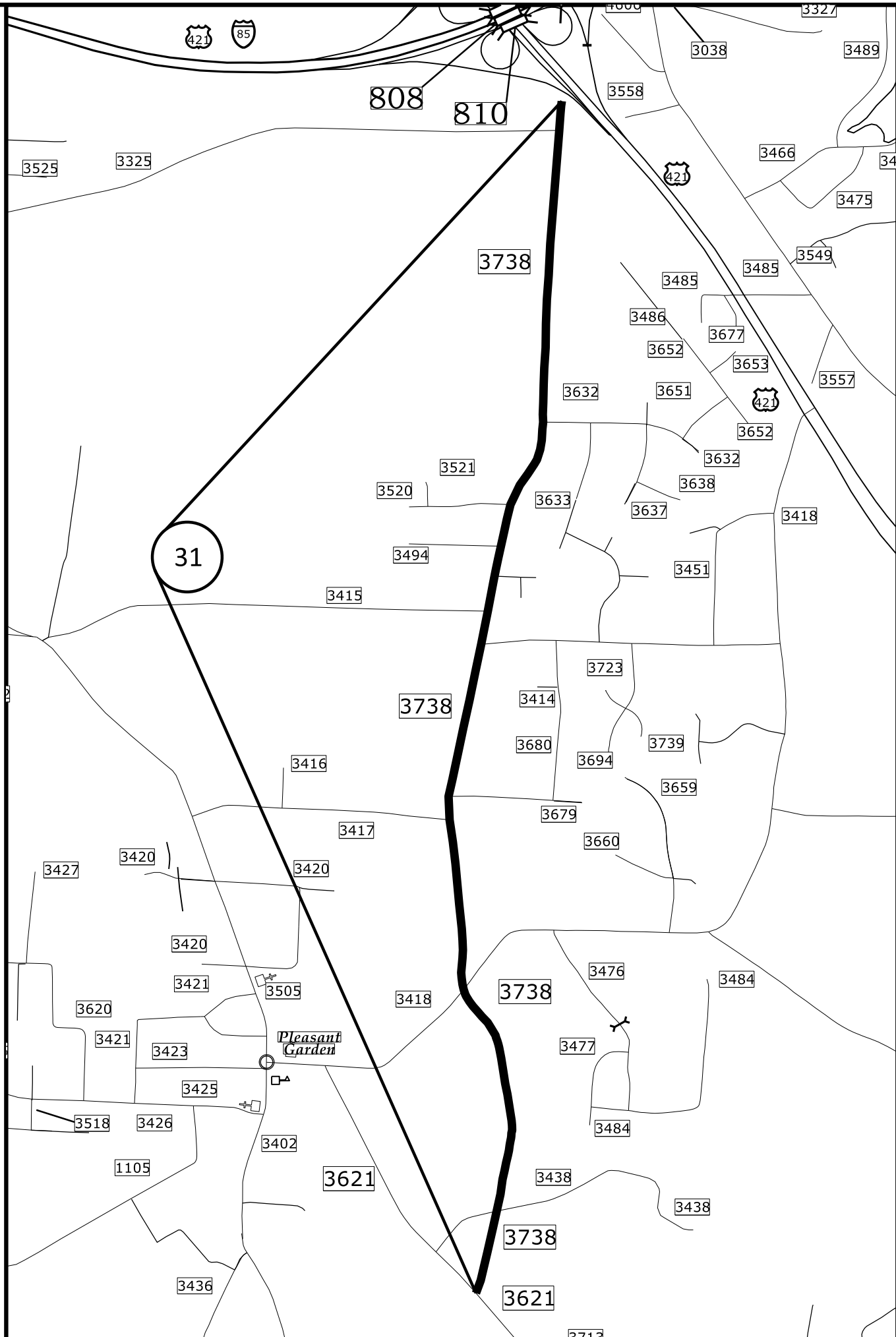
- Map 27 SR 5413 - Hargrove Dr
Skip Paving the intersection at
Map 28 SR 5410 Bradburn Dr.
Tie into the new surface of Map.

- Map 28 SR 5410 - Bradburn Dr
Pave thru intersection at
Map 27 SR 5413 Hargrove Rd
Tie into the new surface of Map

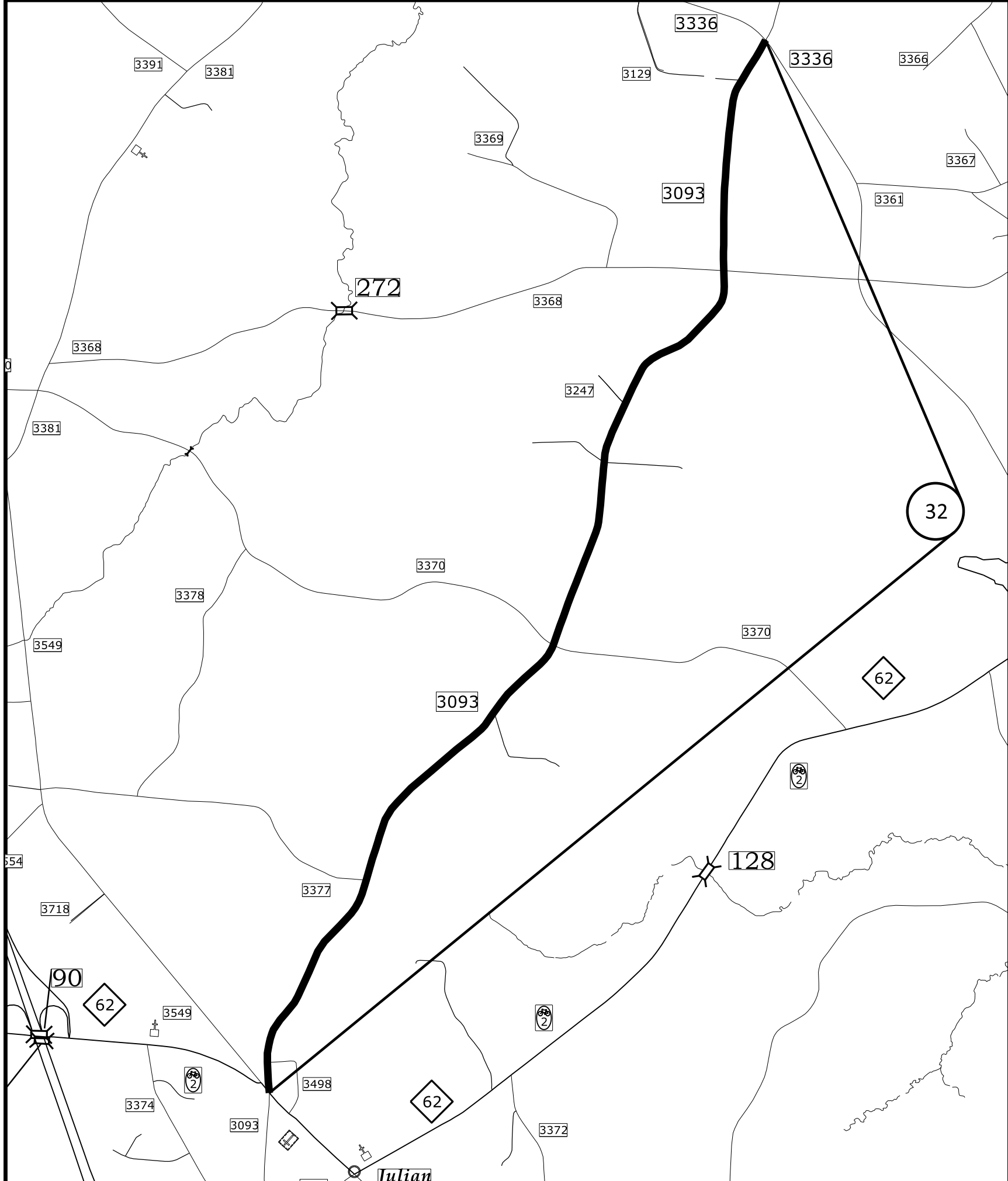
- Map 29 SR 5411 - Dunneman Ct
- Map 30 SR 5412 - Traywick Ct



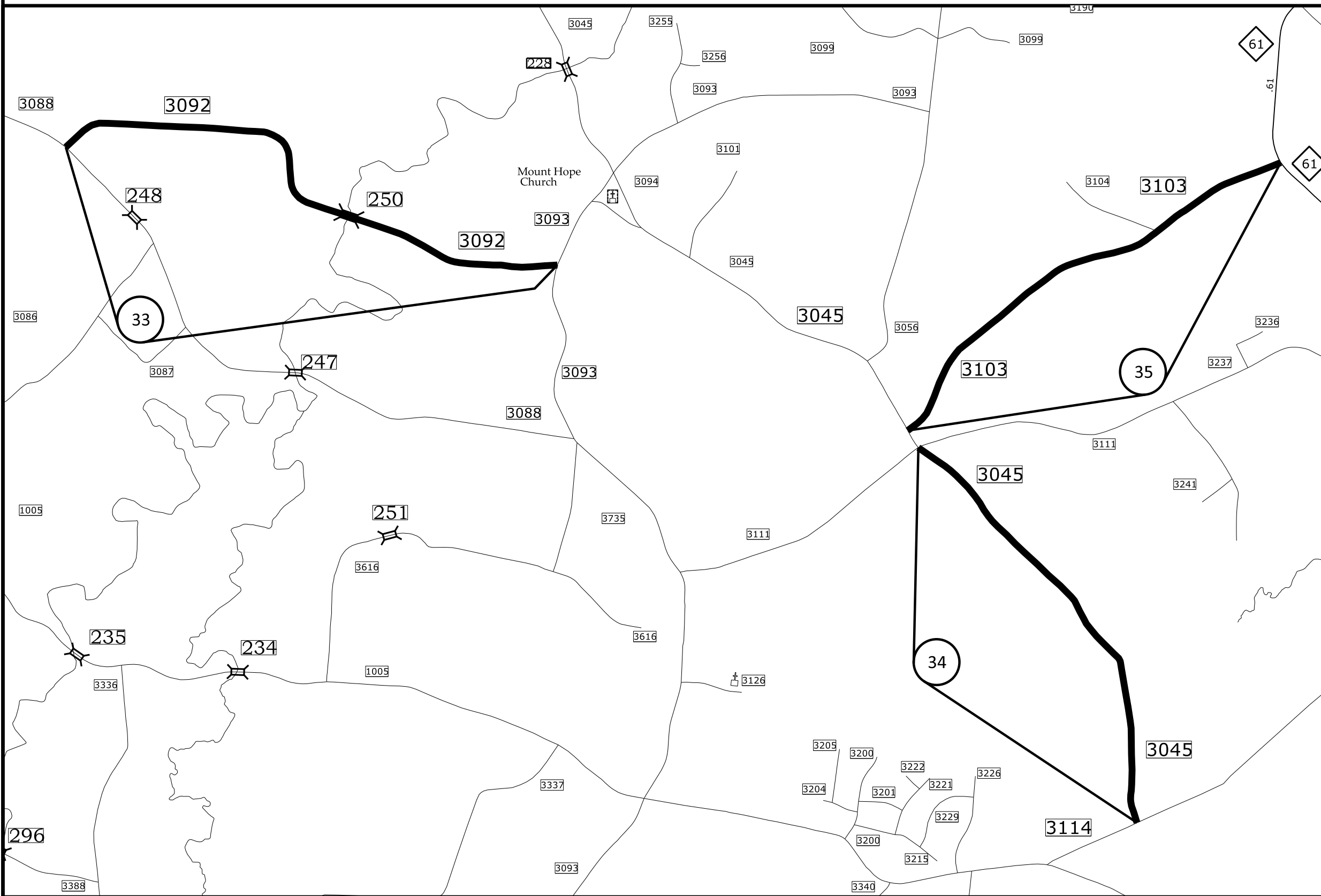
Map 26 SR 3224 - Konica Dr
 Map 37 SR 3144 - Gusenbury Rd



Map 31 SR 3738 - Alliance Church Rd



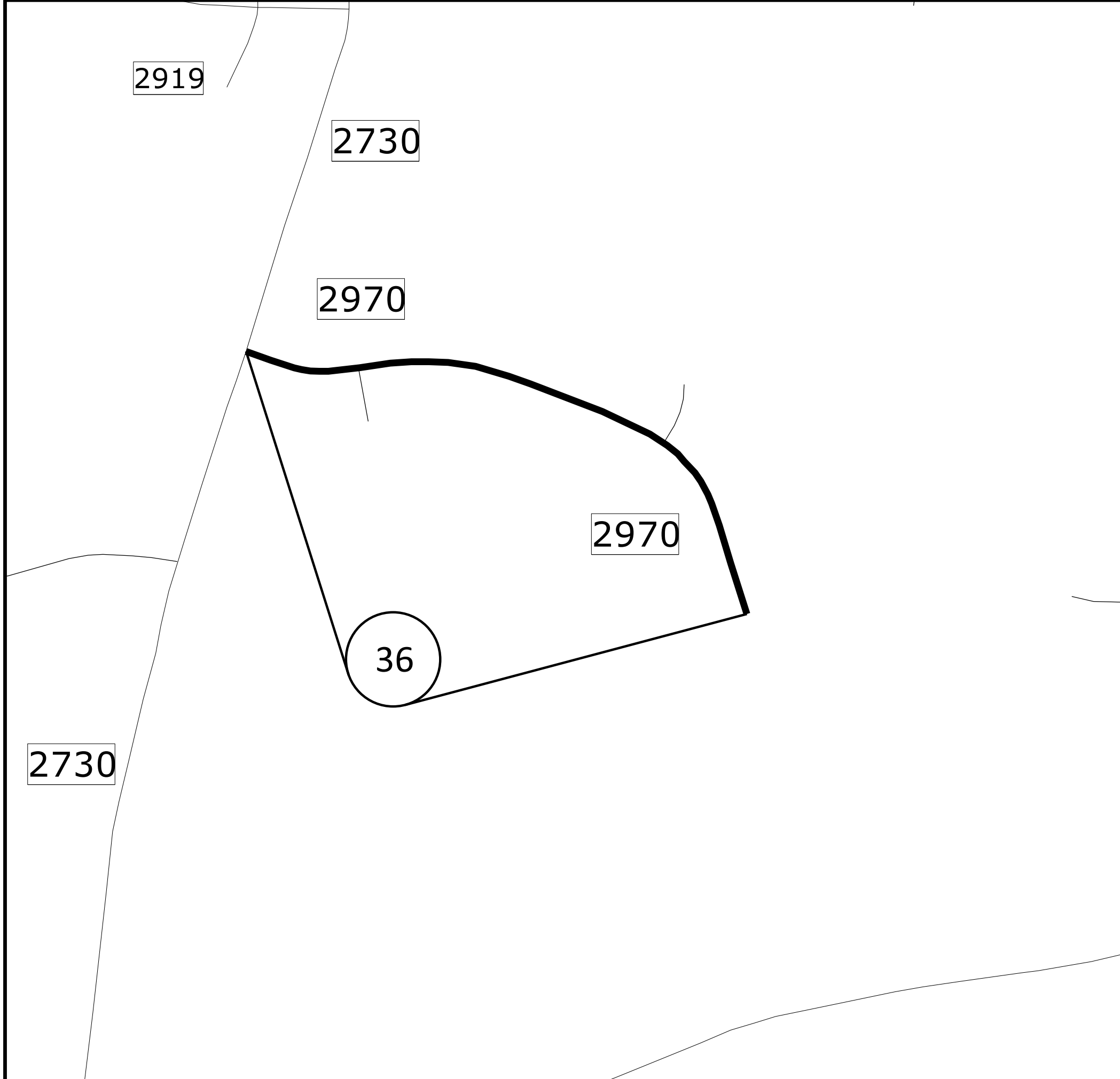
Map 32 SR 3093 - Old Julian Rd



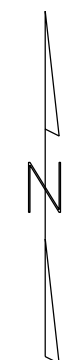
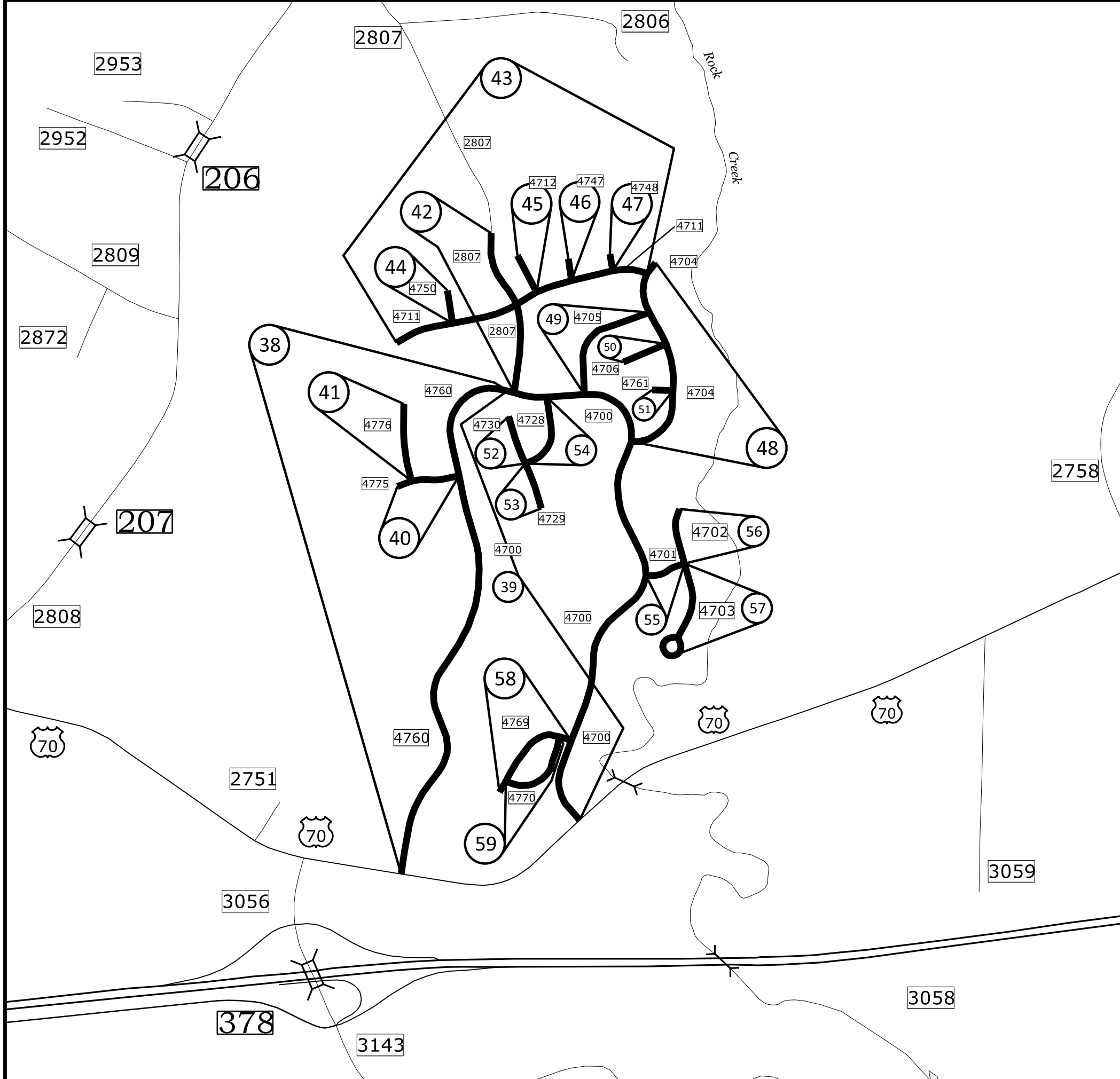
Map 33 SR 3092 - Wild Turkey Rd
Resurface Bridge No. 250
NO Milling on Bridge.

Map 34 SR 3045 - Mt Hope Church Rd
Map 35 SR 3103 - Foster Rd

GUILFORD COUNTY
NORTH CAROLINA



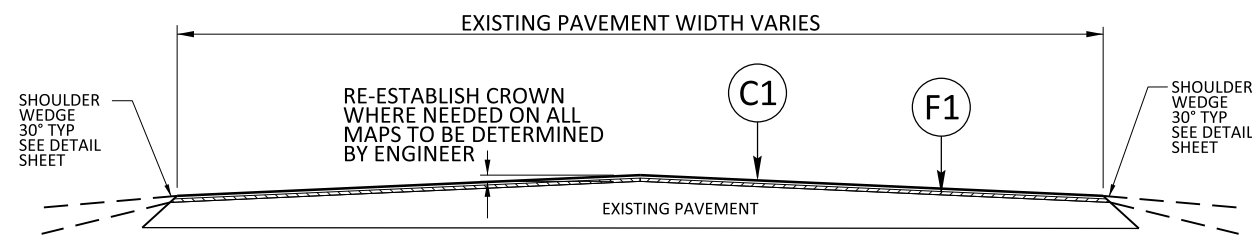
Map 36 SR 2970 - John Washington Rd



- Map 38 SR 4760 - Golf House Road West
- Map 39 SR 4700 - Golf House Road East
- Map 40 SR 4775 - Chesney Way
- Map 41 SR 4776 - Croswell Ct
- Map 42 SR 2807 - Gantwood Ln
Pave through intersection at
Map 43 SR 4711 - Clubside Dr
Tie into new surface of Map

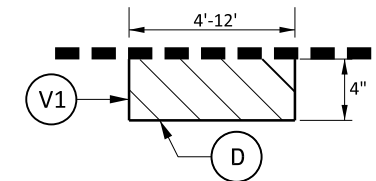
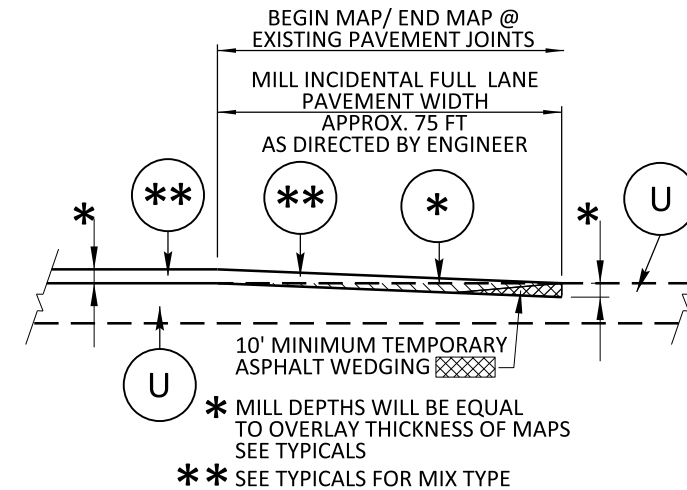
- Map 43 SR 4711 - Clubside Dr
Skip paving through intersection at
Map 42 SR 2807 - Gantwood Ln
Tie into new surface of Map

- Map 44 SR 4750 - Clubside Ct
- Map 45 SR 4712 - Woolwine Ct
- Map 46 SR 4747 - Tigard Ct
- Map 47 SR 4748 - Mckenzie Ct
- Map 48 SR 4704 - Greyrock Rd
- Map 49 SR 4705 - Brookstone Dr
- Map 50 SR 4706 - Barwick Ct
- Map 51 SR 4761 - Thurlow Ct
- Map 52 SR 4730 - Glendevon Ct N
- Map 53 SR 4729 - Glendevon Ct S
- Map 54 SR 4728 - Glendevon Dr
- Map 55 SR 4701 - Haddington Rd
- Map 56 SR 4702 - Haddington Court North
- Map 57 SR 4703 - Haddington Court South
- Map 58 SR 4769 - Wyckshire Ct
- Map 59 SR 4770 - Northwyck Dr



TYPICAL SECTION NO. 1

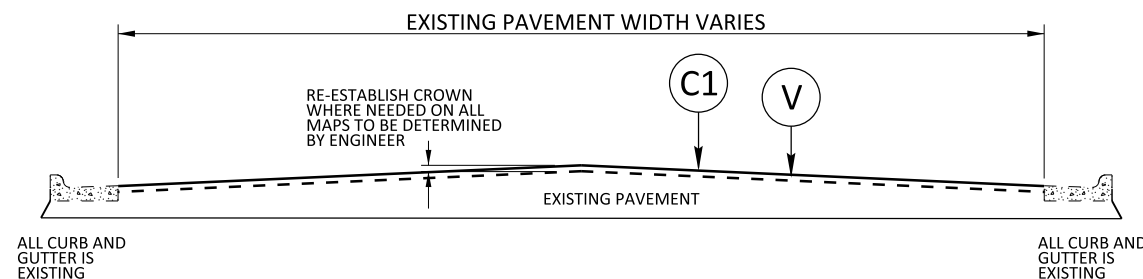
Map 1	SR 2376 - Scottsdale Rd	Map 11	SR 4737 - Benttree Dr	Map 31	SR 3738 - Alliance Church Rd
Map 2	SR 2632 - Larkhill Ct	Map 12	SR 4738 - Bridletree Ct	Map 32	SR 3093 - Old Julian Rd
Map 3	SR 4727 - Begian Dr	Map 13	SR 4739 - Branding Ct	Map 33	SR 3092 - Wild Turkey Rd
Map 4	SR 4741 - Belgian Ct	Map 14	SR 4779 - Hitching Post Dr		Resurface Bridge #250
Map 5	SR 4743 - Iron Weed Dr	Map 15	SR 4780 - Pony Run Dr	Map 34	SR 3045 - Mt Hope Church Rd
Map 6	SR 4726 - Stetson Dr	Map 16	SR 4781 - Pony Run Ct	Map 35	SR 3103 - Foster Rd
Map 7	SR 4725 - Loredo Ct	Map 17	SR 4722 - Saddler Tr	Map 36	SR 2970 - John Washington Rd
Map 8	SR 4724 - Quarter Horse Tr	Map 18	SR 4723 - Saddler Ct	Map 37	SR 3144 - Gusenbury Rd
Map 9	SR 2990 - Saddlebranch Dr	Map 19	SR 4742 - Chestnut Bay Ct		
Map 10	SR 4740 - Saddlebranch Ct	Map 20	SR 2724 - Wagoner Rd		



MILL FILL WITH INTERMEDIATE COURSE, TYPE I19.0C AT LOCATIONS AS DIRECTED BY THE ENGINEER.

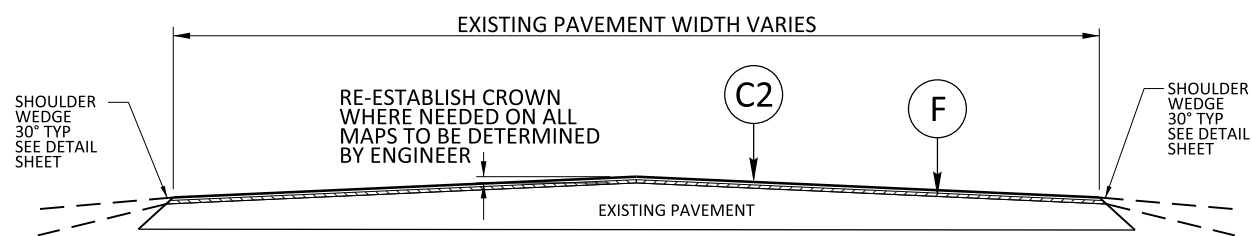
PATCHING EXISTING PAVEMENT DETAIL

INCIDENTAL MILLING AT TIE-IN DETAIL



TYPICAL SECTION NO. 2

Map 21	SR 5416 - Covered Wagon Rd	Map 38	SR 4760 - Golf House Road West	Map 49	SR 4705 - Brookstone Dr
Map 22	SR 5414 - Cragganmore Dr	Map 39	SR 4700 - Golf House Road East	Map 50	SR 4706 - Barwick Ct
Map 23	SR 5415 - Oban Ct	Map 40	SR 4775 - Chesney Way	Map 51	SR 4761 - Thurlow Ct
Map 24	SR 5417 - Laggan Dr	Map 41	SR 4776 - Croswell Ct	Map 52	SR 4730 - Glendevon Ct N
Map 25	SR 5418 - Bowmore Pl	Map 42	SR 2807 - Gantwood Ln	Map 53	SR 4729 - Glendevon Ct S
Map 27	SR 5413 - Hargrove Dr	Map 43	SR 4711 - Clubside Dr	Map 54	SR 4728 - Glendevon Dr
Map 28	SR 5410 - Bradburn Dr	Map 44	SR 4750 - Clubside Ct	Map 55	SR 4701 - Haddington Rd
Map 29	SR 5411 - Dunneman Ct	Map 45	SR 4712 - Woolwine Ct	Map 56	SR 4702 - Haddington Court North
Map 30	SR 5412 - Traywick Ct	Map 46	SR 4747 - Tigard Ct	Map 57	SR 4703 - Haddington Court South
		Map 47	SR 4748 - Mckenzie Ct	Map 58	SR 4769 - Wyckshire Ct
		Map 48	SR 4704 - Greyrock Rd	Map 59	SR 4770 - Northwyck Dr

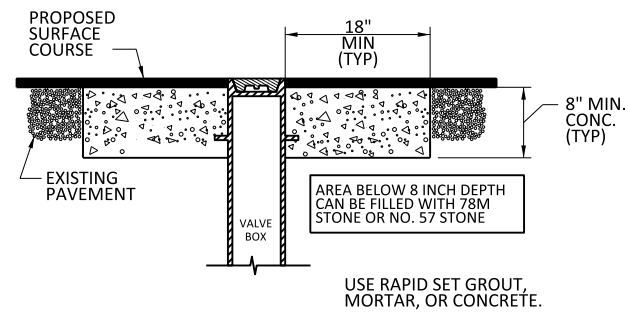


TYPICAL SECTION NO. 3

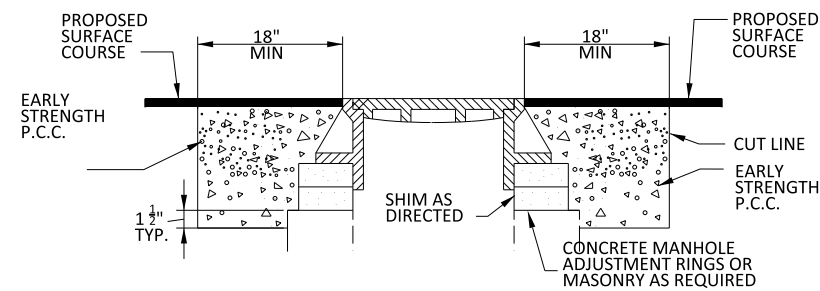
Map 26 SR 3224 - Konica Dr

PAVEMENT SCHEDULE

C1	PROP. APPROX. 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B TO BE APPLIED AT AN AVERAGE RATE OF 137.5 LBS PER SQ YD.
C2	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, TO BE APPLIED AT AN AVERAGE RATE OF 165 LBS PER SQ YD.
D	PROP. APPROX. 4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD.
F	AST MAT COAT, #67
F1	AST MAT COAT, #78M
U	EXISTING PAVEMENT
V	MILL ASPHALT PAVEMENT, 1 1/4" DEPTH
V1	MILL ASPHALT PAVEMENT, 4" DEPTH

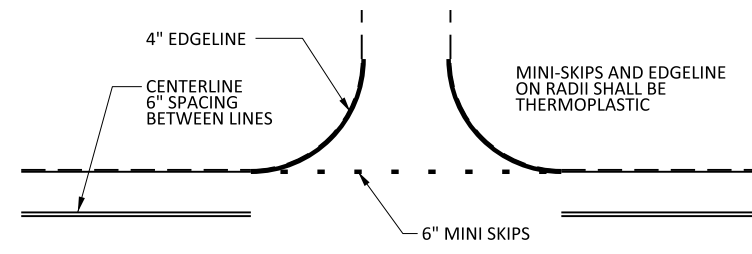


STANDARD CONCRETE ENCASMENT FOR VALVE CASTINGS IN PAVEMENT



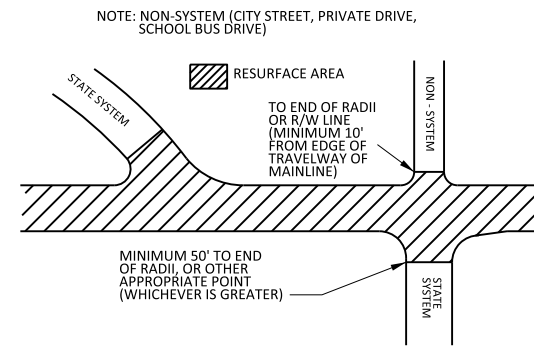
- NOTES:
1. MORTAR SHALL BE MIXED TO NCDOT SPECIFICATIONS.
 2. ALL FAULTY EXISTING BRICKWORK TO BE REMOVED AND REPLACED WITH NEW BRICK MASONRY.
 3. EXCAVATION FOR THE ADJUSTMENT SHALL BE SHEER CUT ON ALL SIDES.
 4. RAPID SET GROUT, MORTAR, OR CONCRETE SHALL BE USED CLASS B CONCRETE MAY BE USED WHEN ADJUSTMENTS ARE NOT IN THE TRAVEL LANE.

STANDARD CONCRETE ENCASMENT FOR MANHOLE CASTINGS IN PAVEMENT

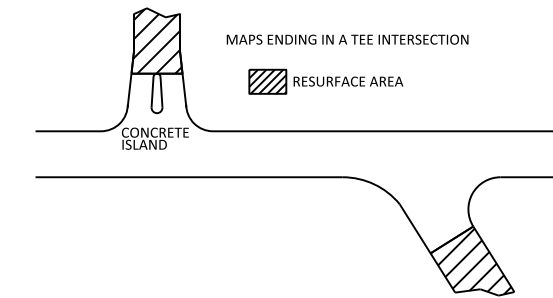


NOTE: MINI SKIPS SHALL BE PLACED ON A 8' CYCLE, CONTAINING A 6' AND 2' SKIP, THE WIDTH OF THE SKIP SHALL BE 6".

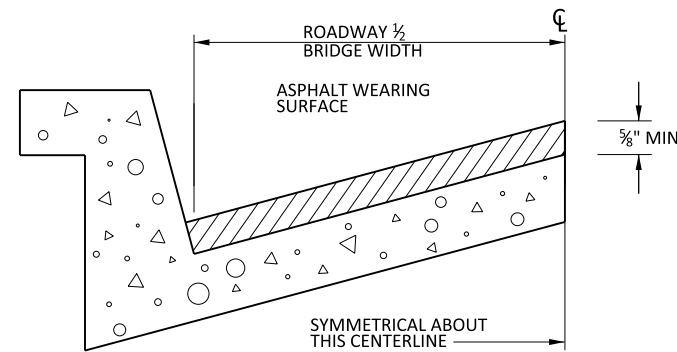
TO BE USED AT ALL NON-SIGNALIZED INTERSECTIONS (NOT TO SCALE)



PAVING DETAIL 1 MAIN LINE IS BEING RESURFACED

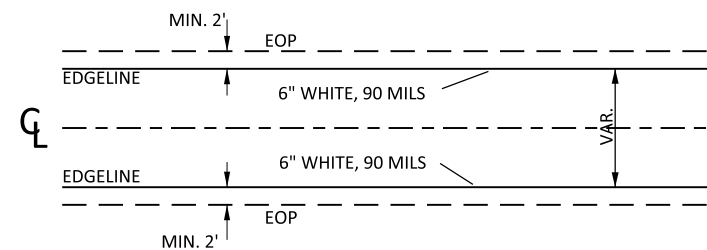


PAVING DETAIL 2 MAIN LINE NOT BEING RESURFACED



BRIDGE HALF TYPICAL SECTION

FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN. THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. A THICKNESS OF NOT LESS THAN 3/8" SHALL BE PROVIDED. THE MAXIMUM THICKNESS SHALL PREFERABLY BE 1-1/2" UNLESS IT IS IMPRACTICAL TO PROVIDE A SMOOTH RIDING SURFACE OTHERWISE.

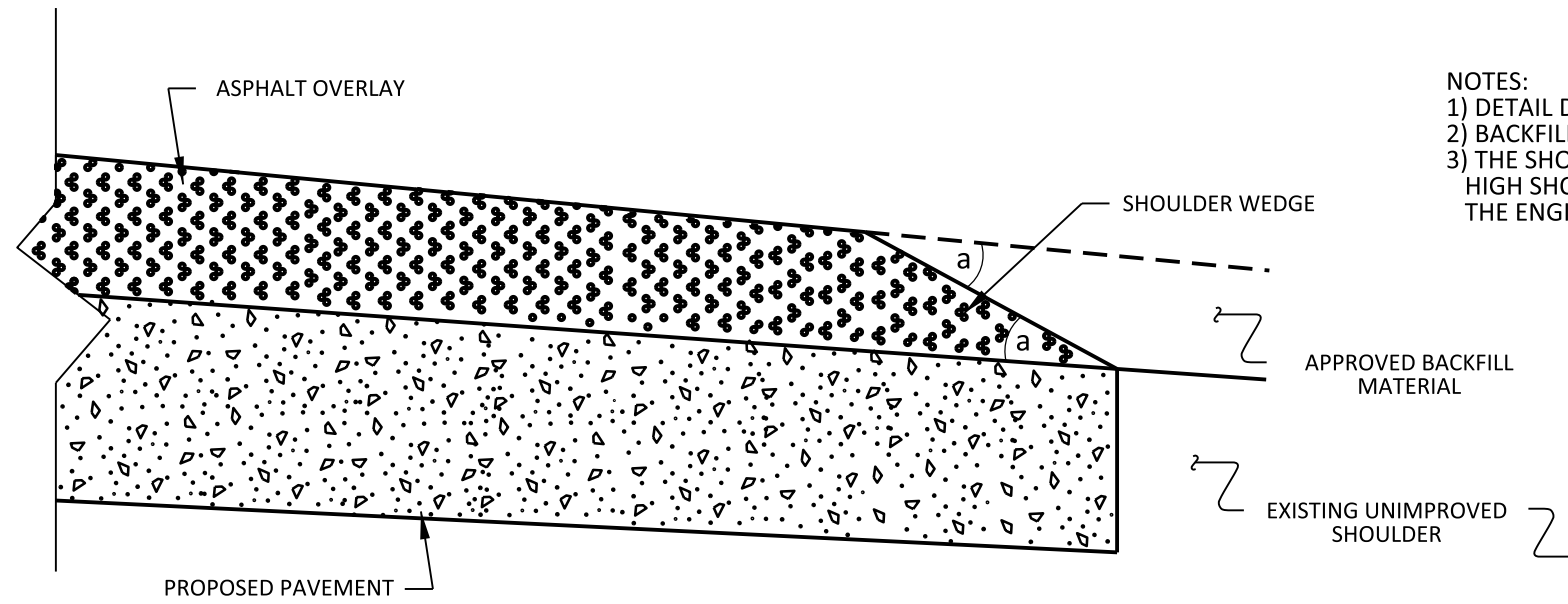


- NOTE:
1. TO BE USED IN CONJUNCTION WITH MAP 3 NC 150.
 2. USE IN CONJUNCTION WITH THE EXISTING PAVEMENT MARKINGS TO ESTABLISH THE STRIPING.
 3. USE IN CONJUNCTION WITH THE NCDOT STANDARD DRAWINGS.

STRIPING DETAIL 1 GENERAL STRIPING DETAIL FOR ENTIRE PROJECT

PAVEMENT SCHEDULE

C1	PROP. APPROX. 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B TO BE APPLIED AT AN AVERAGE RATE OF 137.5 LBS PER SQ YD.
C2	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, TO BE APPLIED AT AN AVERAGE RATE OF 165 LBS PER SQ YD.
D	PROP. APPROX. 4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD.
F	AST MAT COAT, #67
F1	AST MAT COAT, #78M
U	EXISTING PAVEMENT
V	MILL ASPHALT PAVEMENT, 1 1/4" DEPTH
V1	MILL ASPHALT PAVEMENT, 4" DEPTH

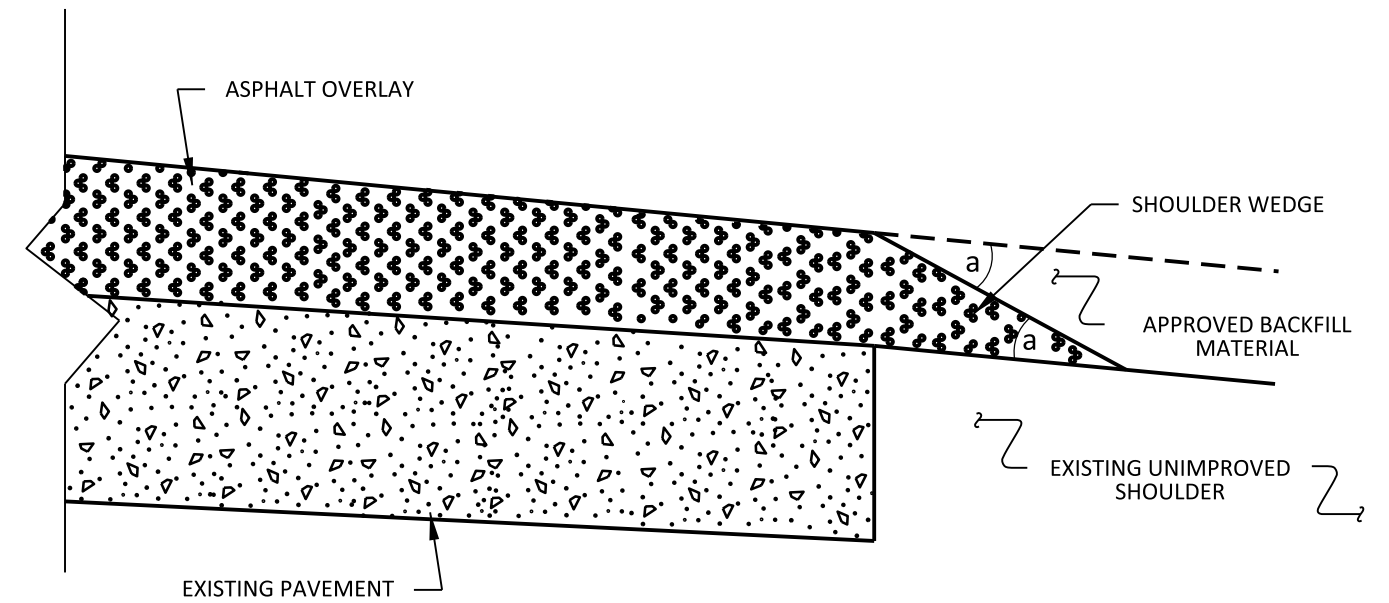


NOTES:

- 1) DETAIL DOES NOT APPLY TO OGAFc AND ULTRA-THIN BONDED WEARING COURSE.
- 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
- 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS DIRECTED BY THE ENGINEER.

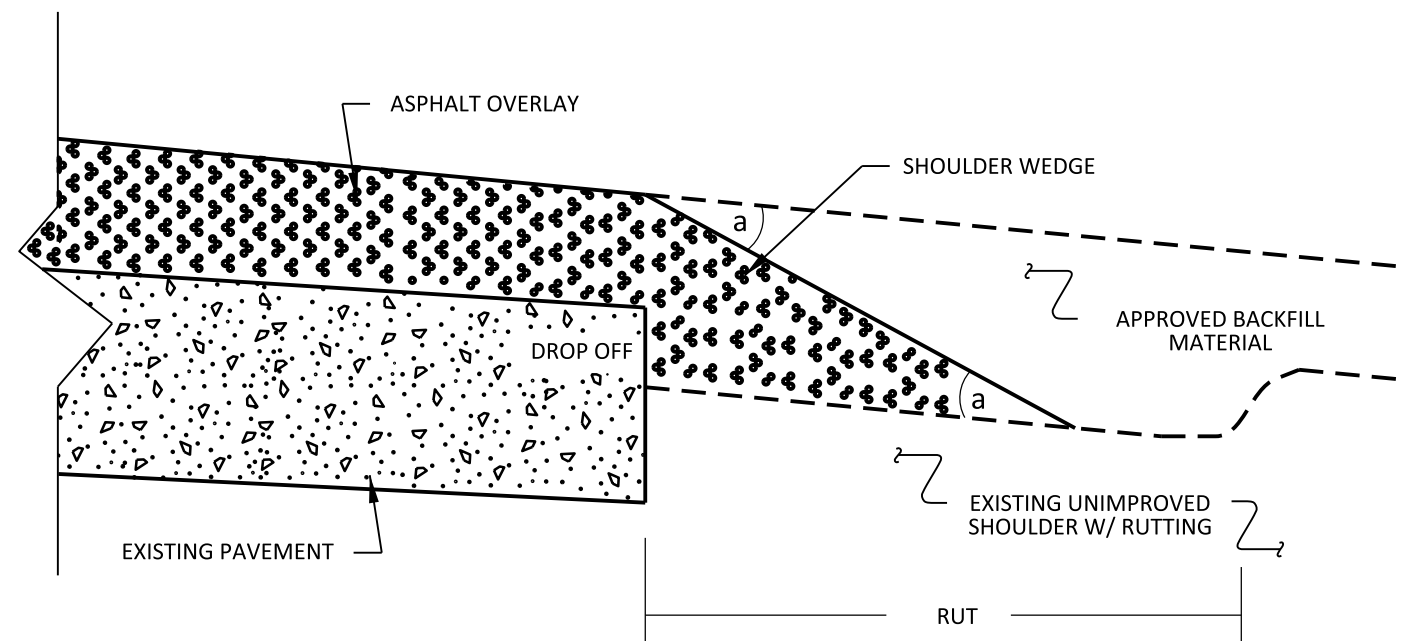
SHOULDER WEDGE DETAIL

(Resurfacing Projects w/ Widening or with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL

(Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL

(Resurfacing Adjacent to Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT
Office 919-707-6950 FAX 919-250-4119

SHOULDER WEDGE DETAILS

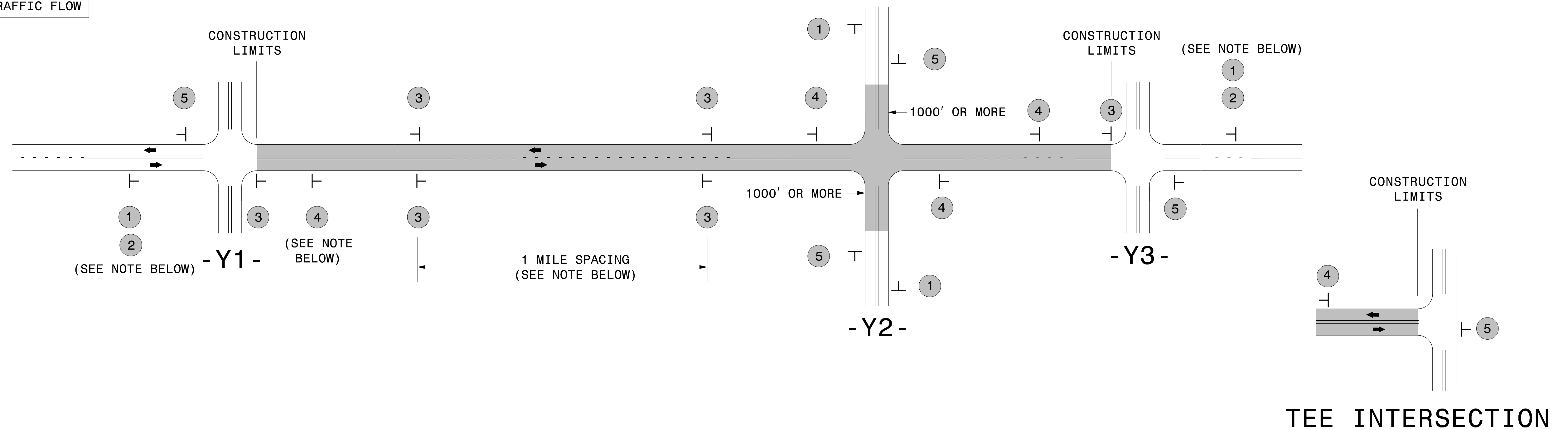
ORIGINAL BY: T.SPELL	DATE: 7-19-11
MODIFIED BY: _____	DATE: 10/16/12
CHECKED BY: _____	DATE: _____
FILE SPEC.: susr/details/stand/shoulderwedge/detail.dgn	

SIGNING FOR RESURFACING PROJECTS

LEGEND

┆ STATIONARY SIGN

← DIRECTION OF TRAFFIC FLOW



MAINLINE (-L-) SIGNING

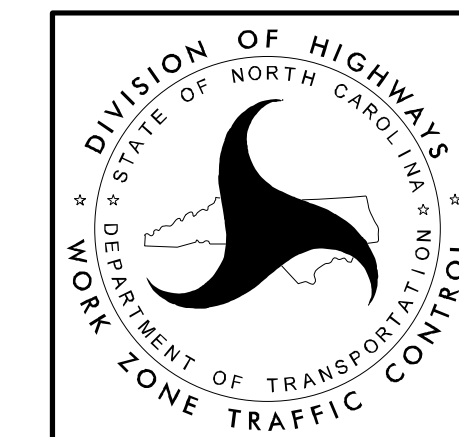
-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1		PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> W20-1 48" X 48" PLACED 500' IN ADVANCE OF FLAGGER. </div> <div style="text-align: center;"> W20-7 A 48" X 48" PLACED 250' IN ADVANCE OF FLAGGER. </div> </div>
	2		#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3		- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.	
	4		- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.	
	5		PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

MAPS LESS THAN 2 MILES

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.



ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING

NOTES

- OVERLAP SAW CUTS AT CORNERS AND INTERSECTION POINTS TO ENSURE UNIFORM SAW SLOT DEPTH.
- MAINTAIN 12" SPACING BETWEEN LOOP WIRE TAIL SECTIONS.
- WIRE LOOPS CONNECTED TO THE SAME DETECTOR IN SERIES.
- LOCATE LOOPS IN CENTER OF LANES UNLESS OTHERWISE SHOWN ON PLANS.
- USE A SERIES OF ONE INCH PIECES OF BACKER ROD SPACED ONE FOOT APART ALONG THE ENTIRE LENGTH OF THE FEEDER SLOT AND LOOP SAW SLOT.
- CONSULT LOOP SEALANT MANUFACTURER TO DETERMINE CURING TIME REQUIRED PRIOR TO MILLING.
- REFER TO STANDARD DRAWING 1725.01 SHEETS 2 AND 3 FOR ADDITIONAL REQUIREMENTS.

SAW SLOT DEPTH CHART
ASSUMING 2" MILLING DEPTH

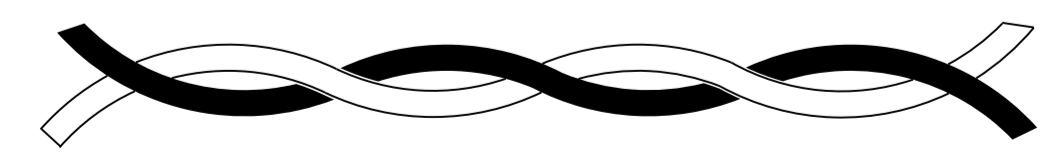
DEPTH (IN)	MAX NO. OF WIRE LAYERS				
	2	3	4	5	6
SAW SLOT DEPTH	4.0	4.5	5.0	5.0	5.0
MINIMUM TOTAL ASPHALT DEPTH REQUIRED	5.0	5.5	6.0	6.0	6.0

LOOP WIRE TWISTING METHOD

INCORRECT WAY TO TWIST WIRE

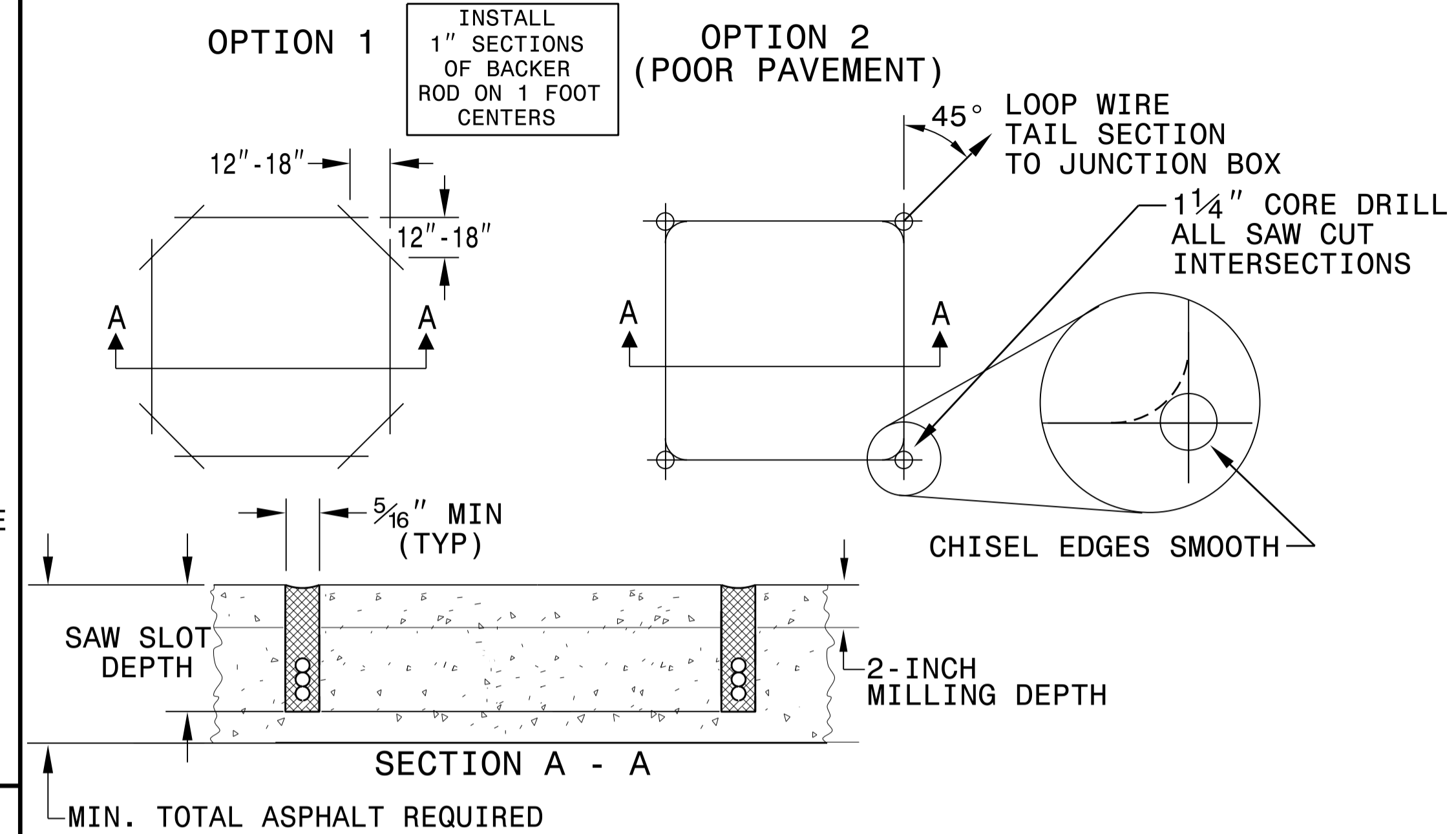


CORRECT WAY TO TWIST WIRE

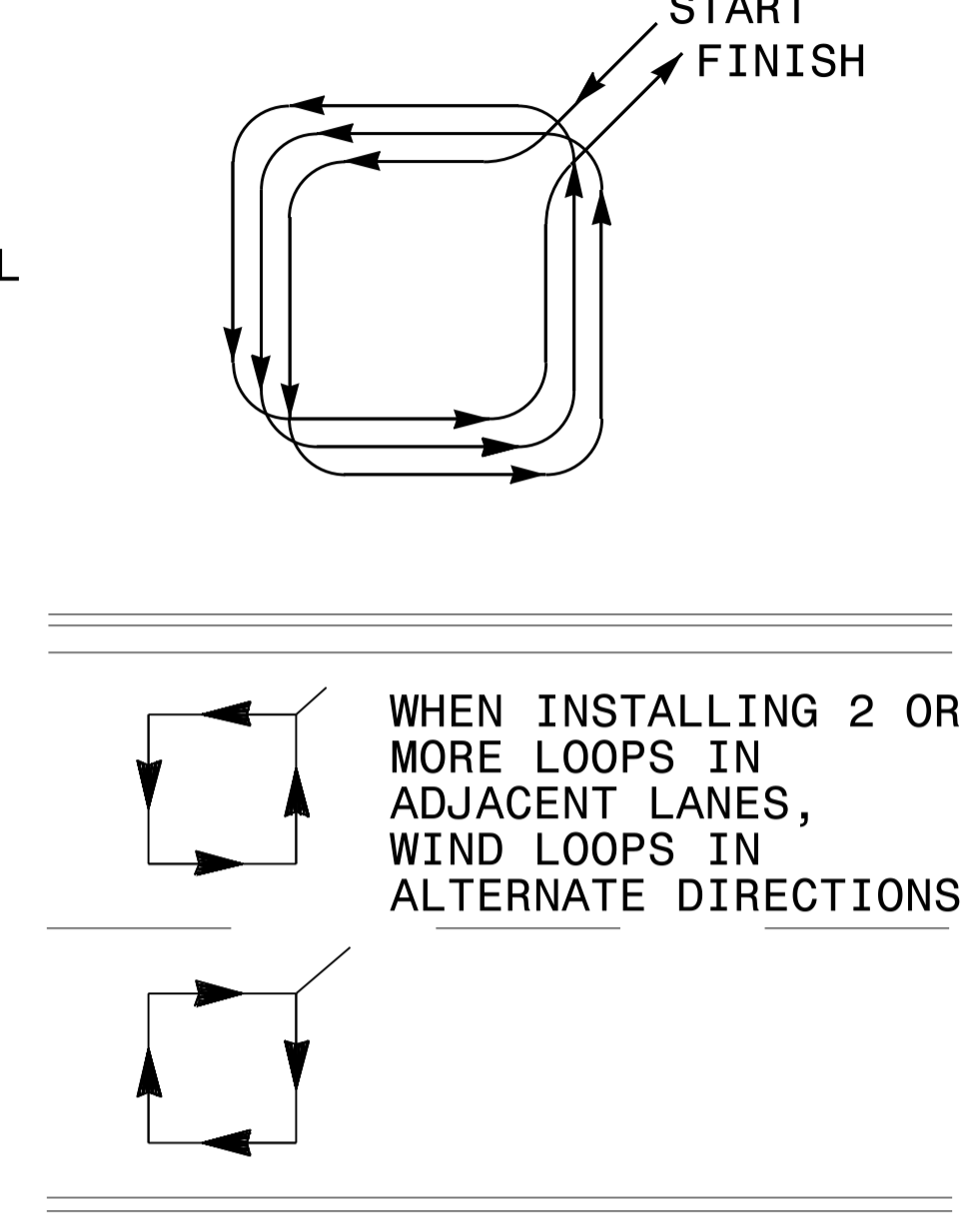


CONVENTIONAL 4-SIDED LOOP

SAW CUT OPTIONS

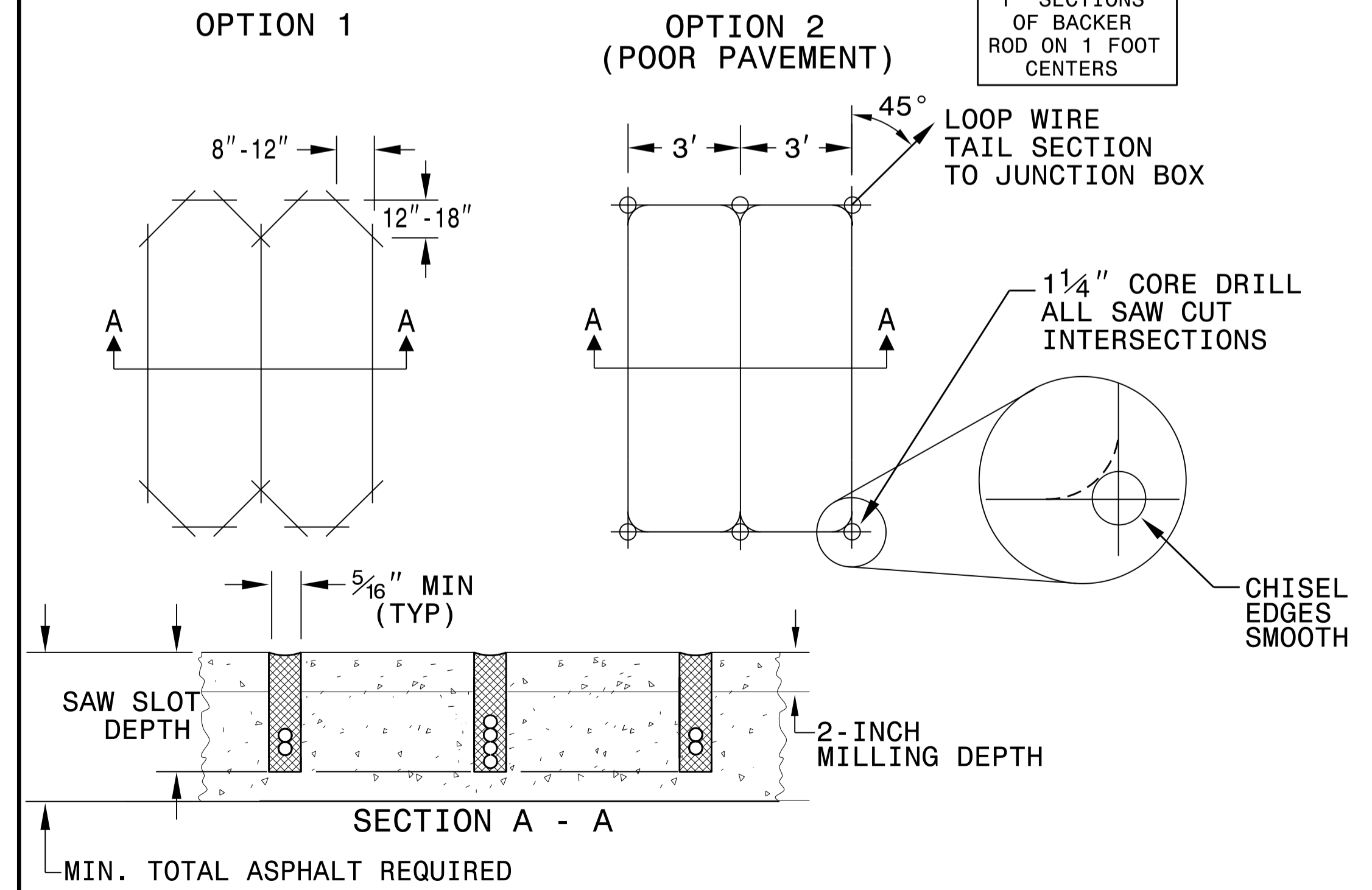


LOOP WINDING METHOD

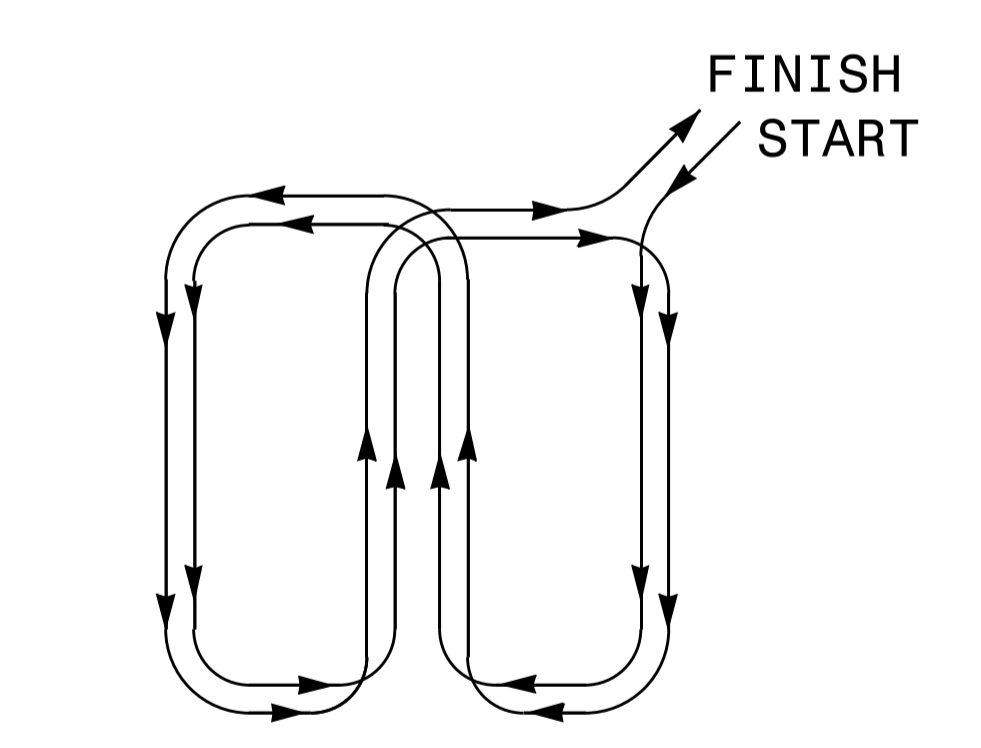


QUADRUPOLE LOOP

SAW CUT OPTIONS

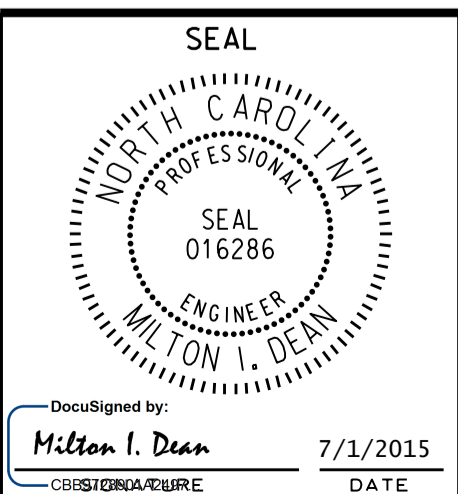
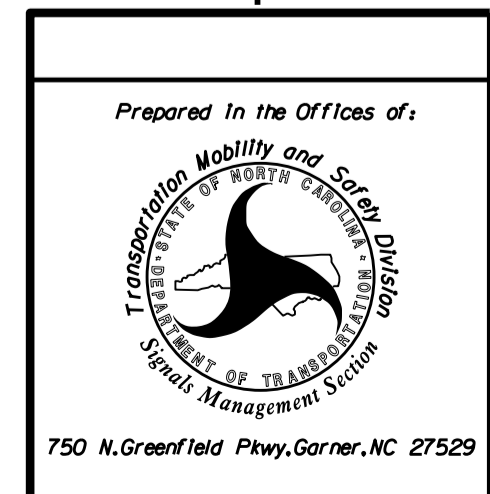


LOOP WINDING METHOD

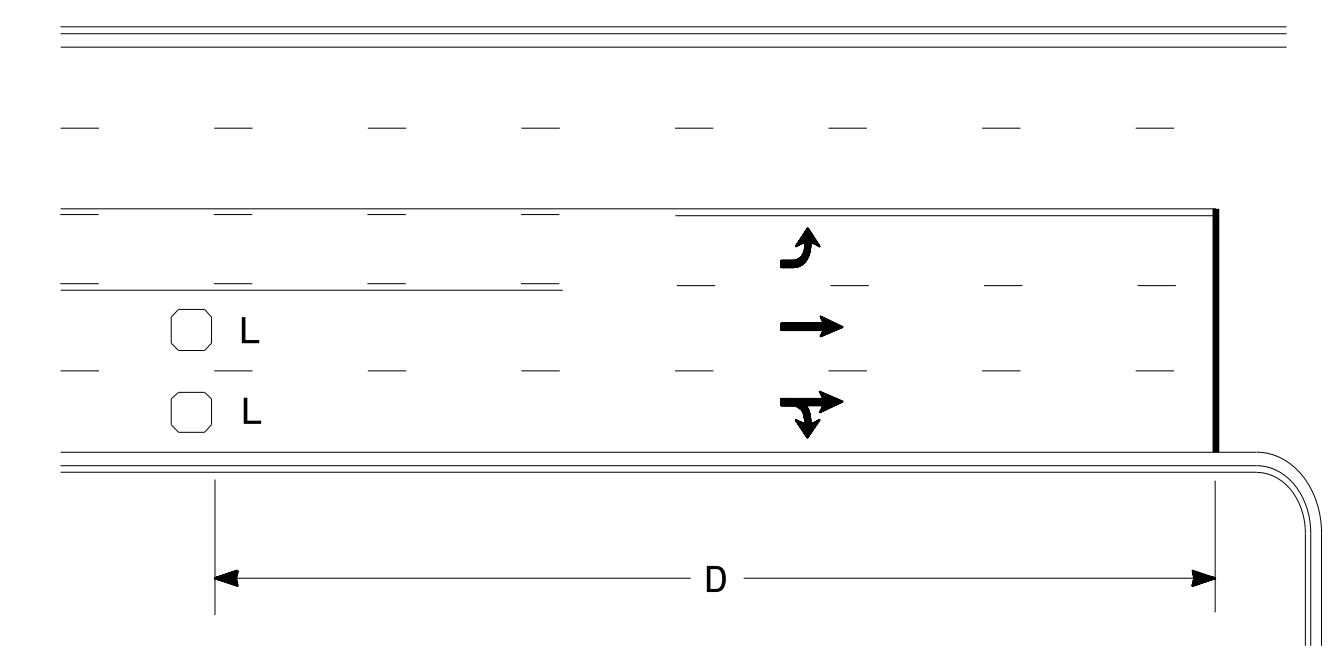


REVISIONS

1. REMOVED TWISTING NOTES FROM TAIL SECT. TO JUNCTION BOX. 2/26/08 MWH
2. REVISED SECTION A - A DETAILS. 6/29/15 JTP



High Speed Detection (≥40 mph)

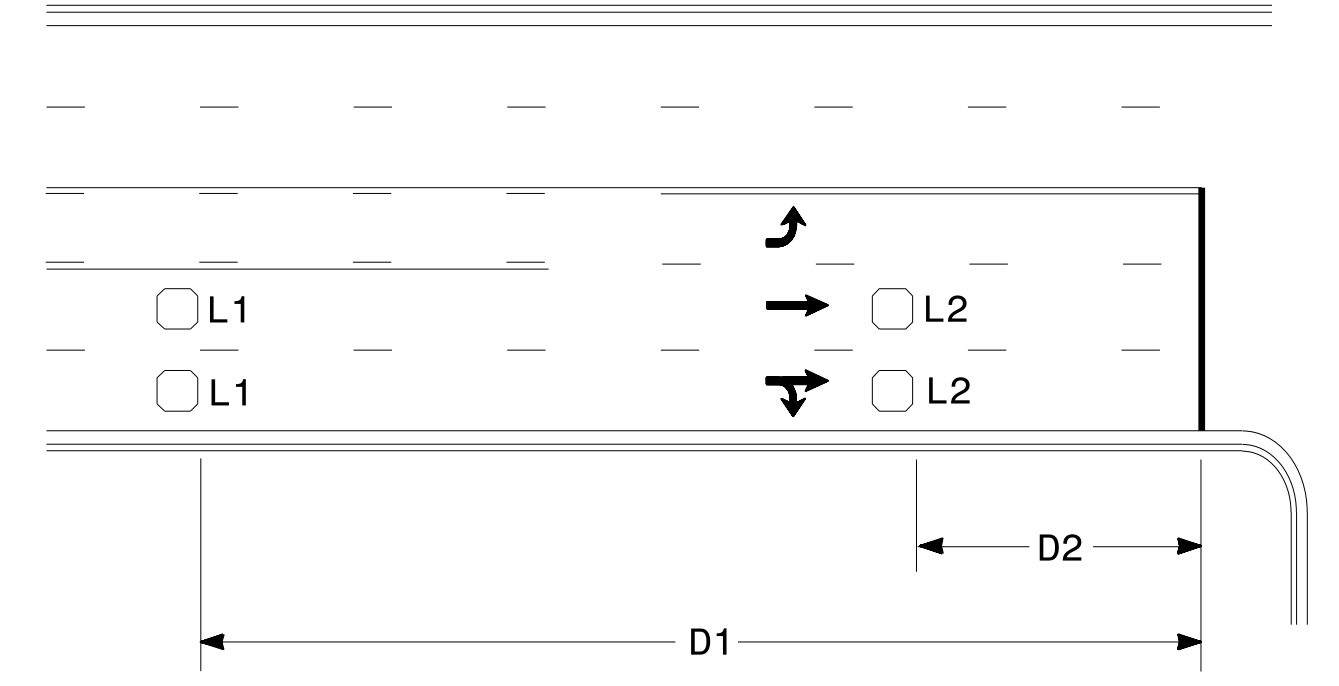


Speed Limit mph	D ft
40	250
45	300
50	355
55	420

L = 6ft X 6ft
Wired in series for TS1
Controllers
Wired separately for TS2,
170, and 2070L Controllers

Volume Density Operation

OR

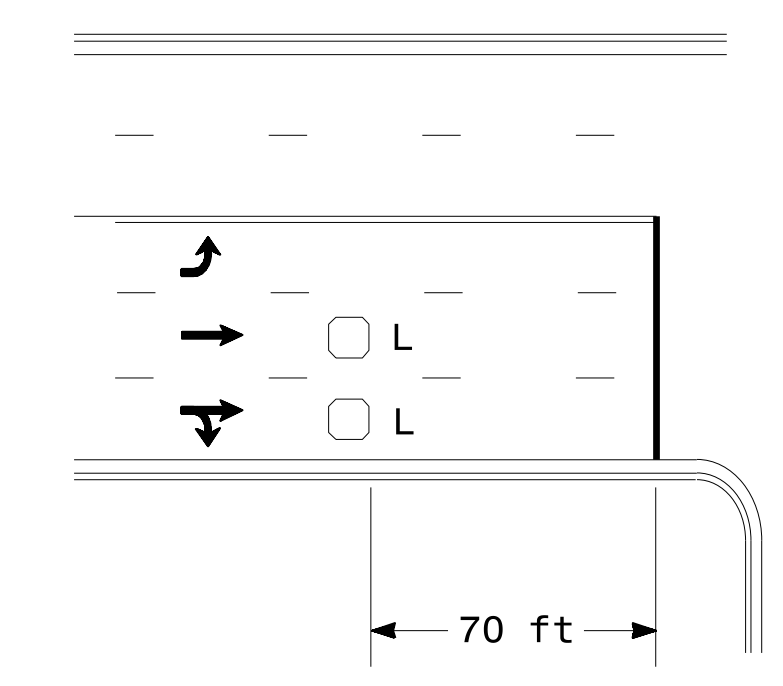


Speed Limit mph	D1 ft	D2 ft
40	250	80
45	300	90
50	355	100
55	420	110

L1 = 6ft X 6ft
Wired in series
L2 = 6ft X 6ft
Wired in series

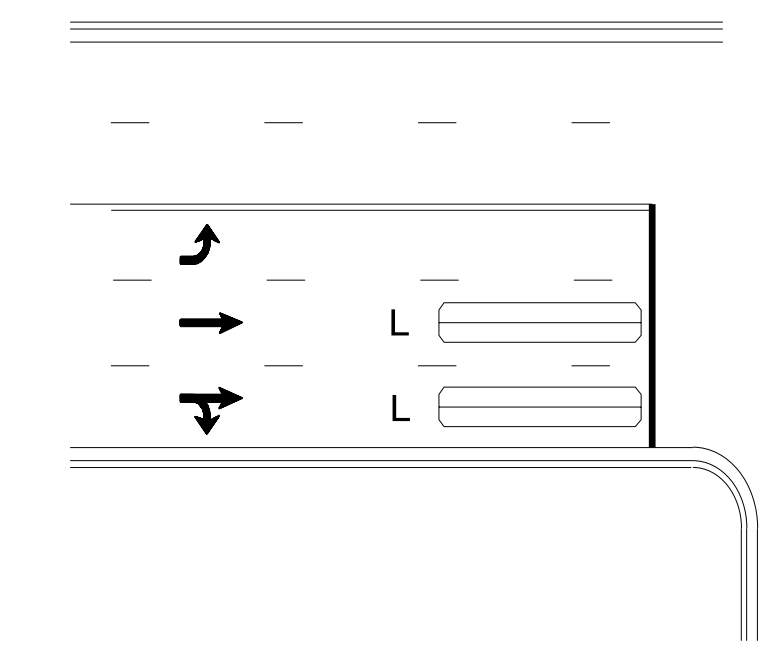
"Stretch" Operation

Low Speed Detection (≤35 mph)



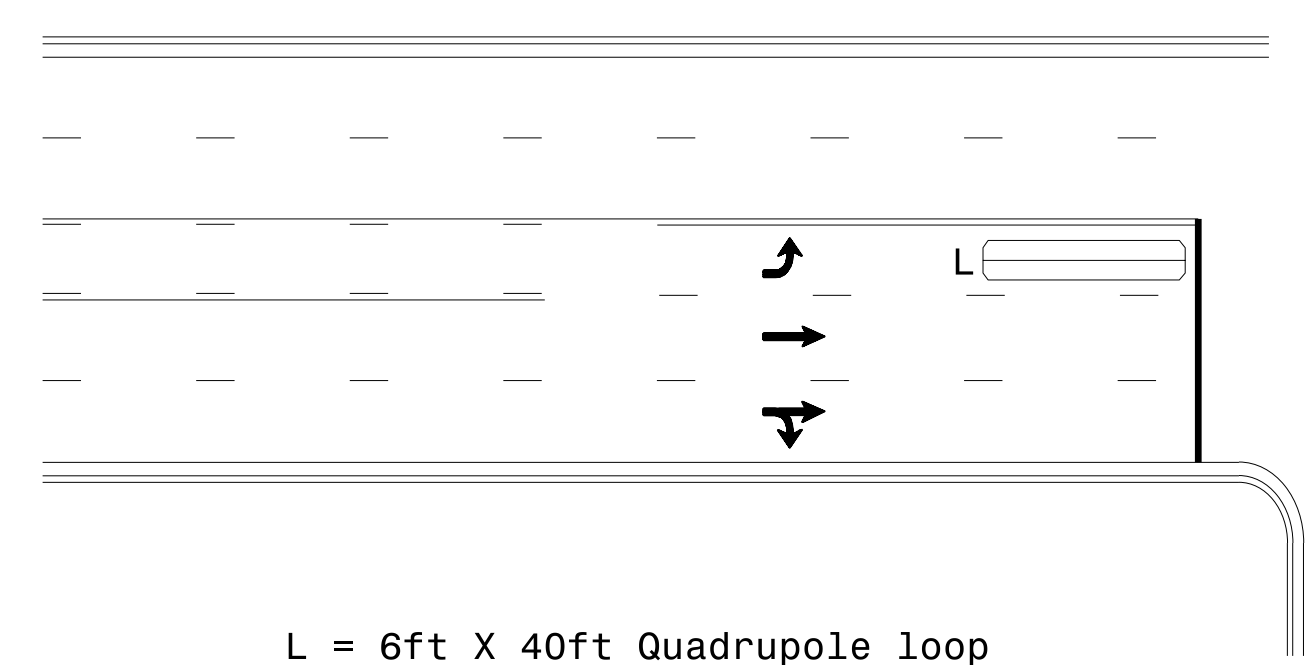
L = 6ft X 6ft
Wired in series

OR



L = 6ft X 40ft
Quadrupole loop, wired separately

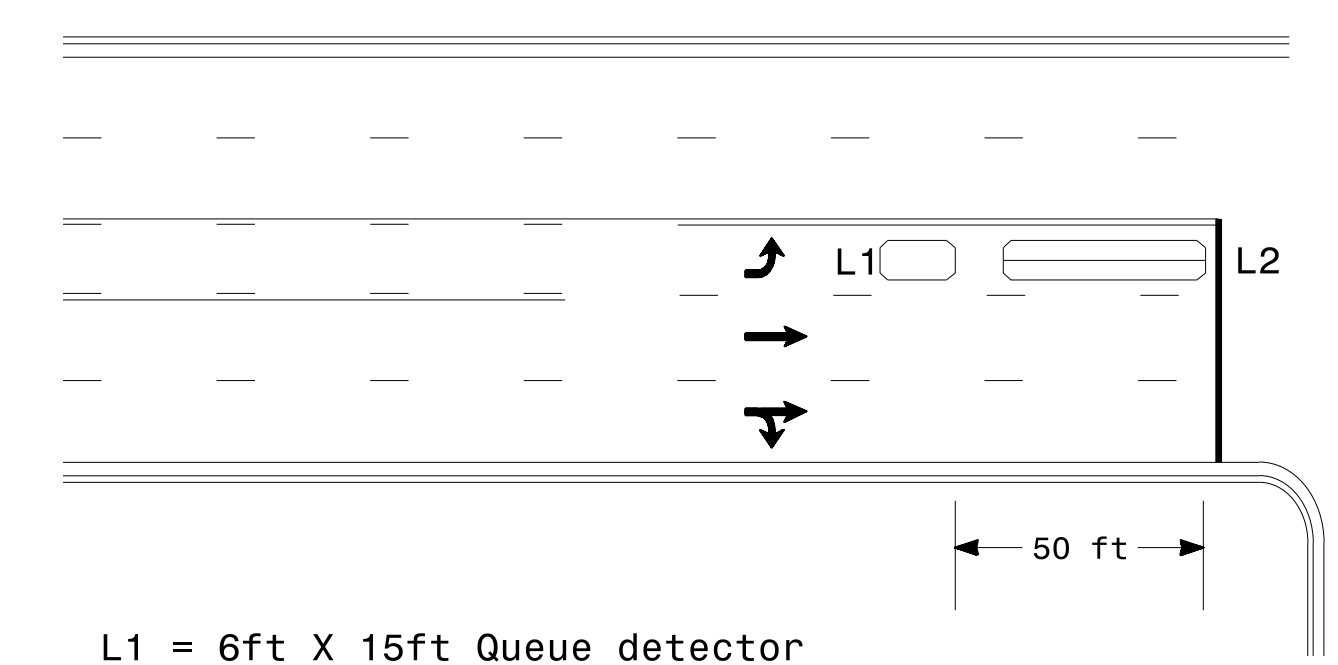
Left Turn Lane Detection



L = 6ft X 40ft Quadrupole loop

Presence Loop Detection

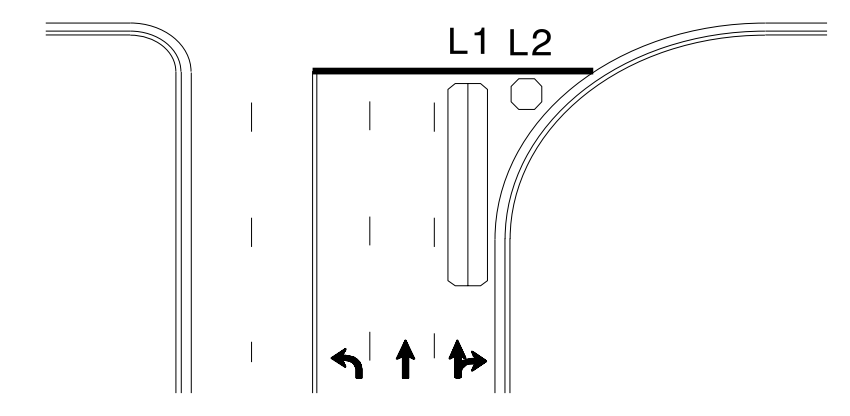
OR



L1 = 6ft X 15ft Queue detector
L2 = 6ft X 40ft Quadrupole loop

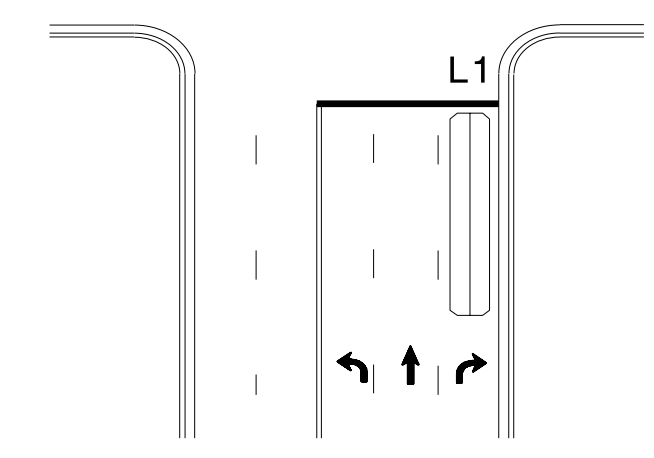
Queue Loop Detection

Right Turn Lane Detection

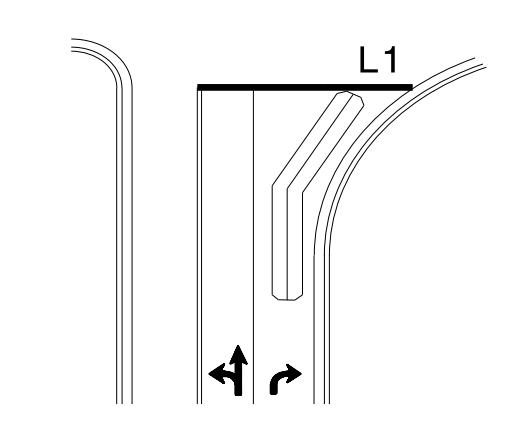


Shared Lane/
Wide Radius Turn

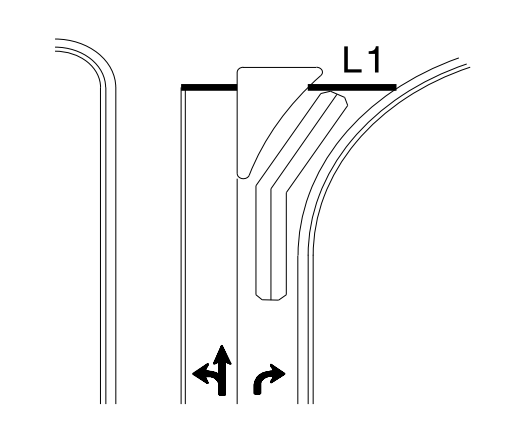
L1 = 6ft X 40ft Quadrupole loop
L2 = 6ft X 6ft [Minimum] Presence loop
Wired separately



Standard Turn

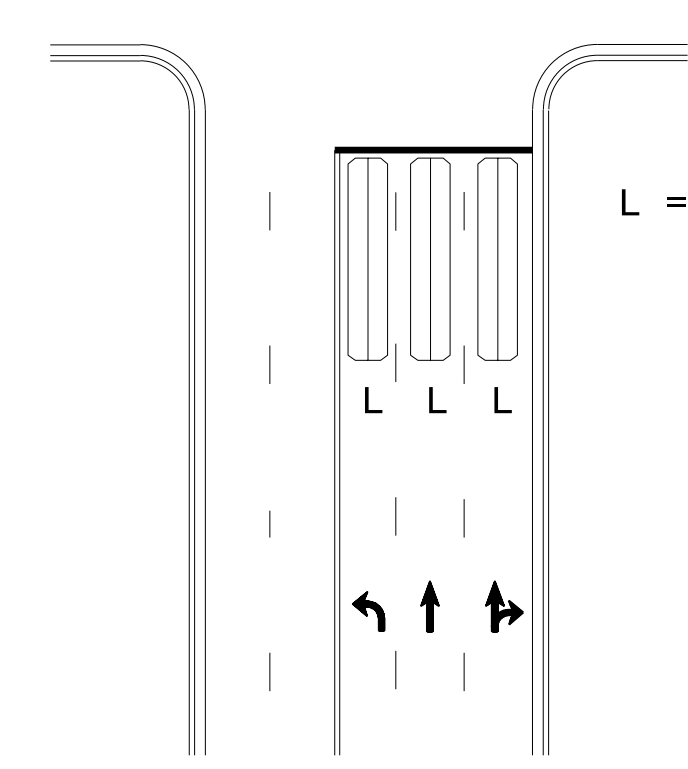


Wide Radius Turn



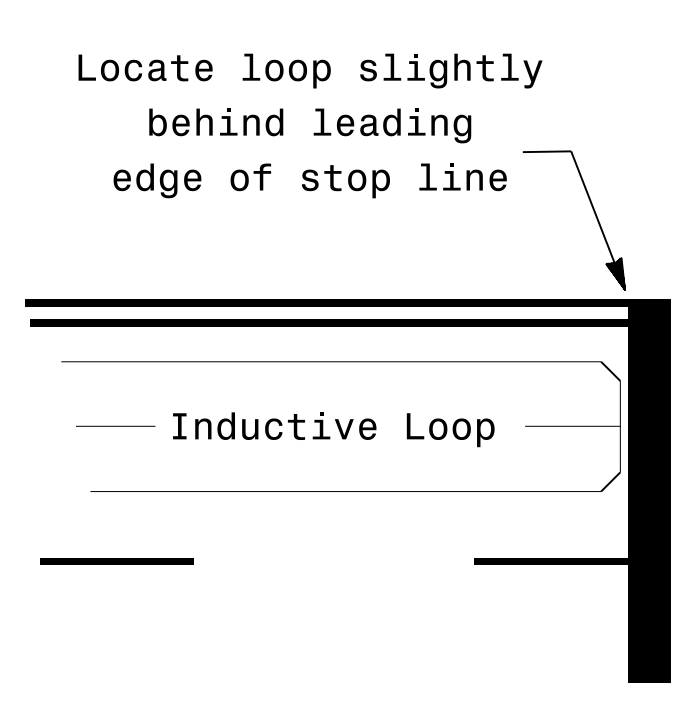
Channelized Turn

Side Street Detection



L = 6ft X 40ft
Quadrupole loop
Wired to separate
detectors/channels

Presence Loop Placement at Stop Lines



Locate loop slightly
behind leading
edge of stop line

- Note:
Loop may be located in advance
of stop line under any of the
following conditions:
- 1) stop line is greater than 15' from edge of intersecting roadway
 - 2) loop detects a permissive or protected/permissive left turn
 - 3) for an exclusive right turn lane

Recommended Number of Turns

Single 6' X 6' loop
(when wired separately):

Length of Lead-in ft	Number of Turns
< 250	3
250-375	4
375-525	5
> 525	6

Quadrupole loops: Use 2-4-2 turns
6' X 15' Loops:
Lead-in < 150', use 2 turns
Lead-in > 150', use 3 turns

750 N. Greenfield Pkwy, Garner, NC 27529

Typical Signal Loop Locations

PLAN DATE: January 2015	REVIEWED BY: JPG
PREPARED BY: PLA	REVIEWED BY:
REVISIONS	INIT. DATE

SEAL
NORTH CAROLINA
PROFESSIONAL ENGINEER
PAMELA L. ALEXANDER
23489

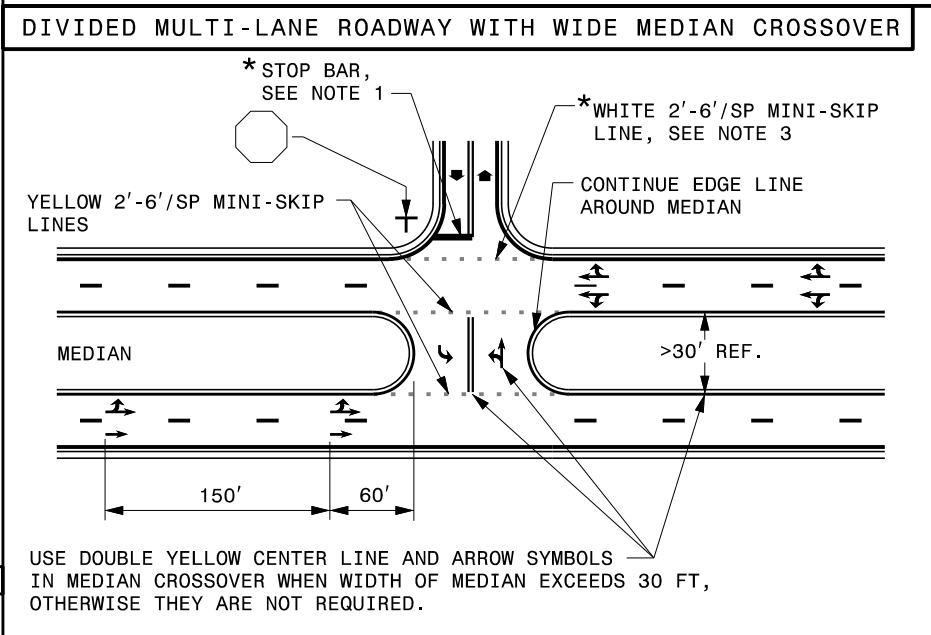
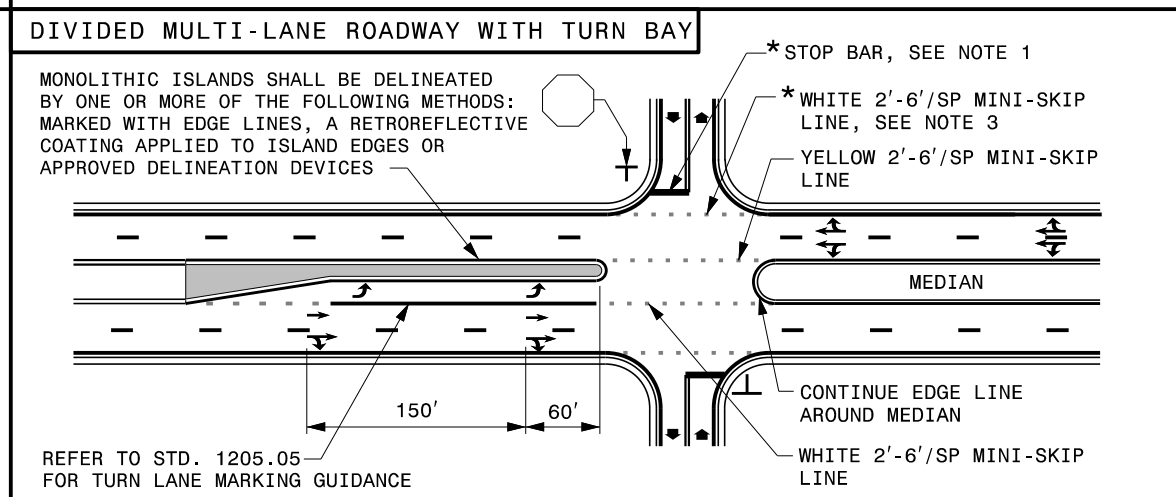
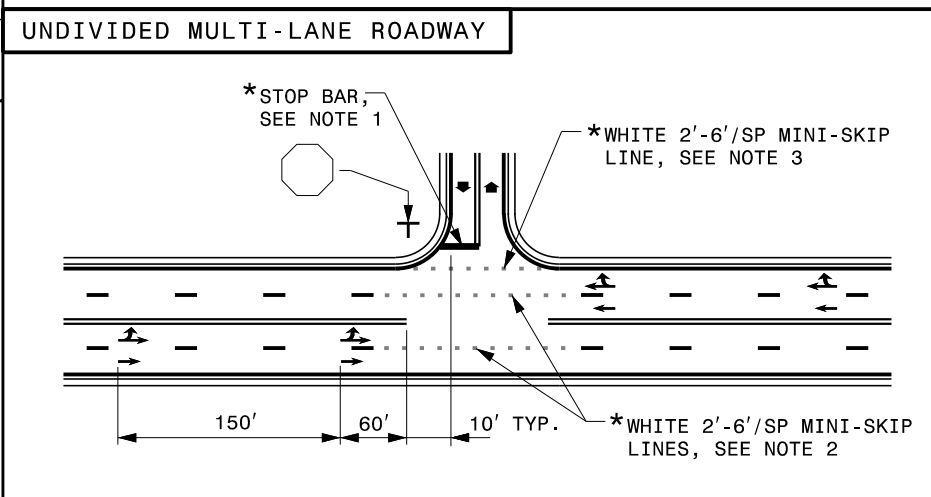
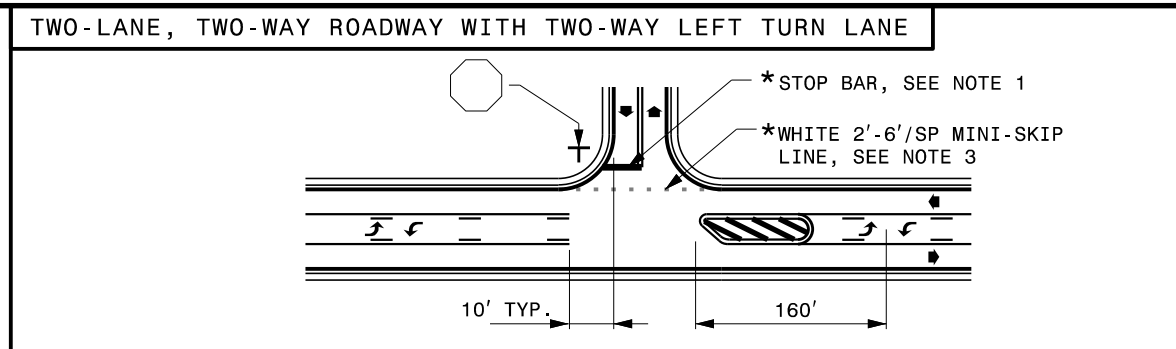
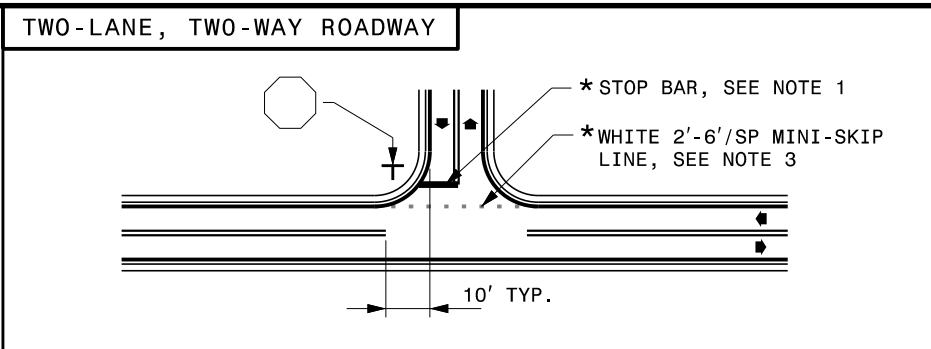
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P. Alexander
1/30/2015 10:44:44 AM
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DATE
SIG. INVENTORY NO.

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 paalexander

DocuSigned by:
Matthew V. Springer
APPROVED: 8/13/2019
DATE: 8/13/2019



STATE OF NORTH CAROLINA
 DEPT. OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 RALEIGH, N.C.
 8-19
 ENGLISH DETAIL DRAWING FOR
PAVEMENT MARKINGS
 INTERSECTIONS
 SHEET 2 OF 2
1205D04



GENERAL NOTES:

- 1- PLACEMENT OF STOP BARS AT NON-SIGNALIZED INTERSECTIONS IS OPTIONAL AND USED WHERE IT IS IMPORTANT TO INDICATE THE POINT WHICH VEHICLES ARE REQUIRED TO STOP. PLACE STOP BARS NO LESS THAN 4 FEET AND NO MORE THAN 30 FEET FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY. USE 10 FEET AS THE TYPICAL SETBACK DISTANCE OR AS DIRECTED BY THE ENGINEER.
- 2- MINI-SKIP LANE LINE EXTENSIONS SHOULD BE USED AT INTERSECTIONS THAT HAVE REDUCED VISIBILITY CONDITIONS SUCH AS OFFSET, SKEWED, OR CURVED ROADWAYS.
- 3- MINI-SKIP EDGE LINE EXTENSIONS MAY BE PLACED THROUGH INTERSECTIONS AND MAJOR DRIVEWAYS.
- 4- REFER TO ROADWAY STANDARD DRAWINGS 1205.01, 1205.02, 1205.05, 1205.08 AND 1205.09 FOR ADDITIONAL PAVEMENT MARKING GUIDANCE.

LEGEND

	STOP SIGN		STATIONARY SIGN
	DIRECTION OF TRAFFIC FLOW		PAVEMENT MARKING SYMBOLS
*	OPTIONAL		

SHEET 2 OF 2
1205D04

STATE OF NORTH CAROLINA
 DEPT. OF TRANSPORTATION
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PAVEMENT MARKINGS
 INTERSECTIONS
 SHEET 2 OF 2
1205D04

REVISED PAVEMENT MARKING ROADWAY STANDARD DRAWING

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	122000000-E	129700000-E	133000000-E	151900000-E	157500000-E	170400000-E	177500000-E	183800000-E	283000000-N	284500000-N	744800000-E		
												MI	FT	TONS	SY	SY	TONS	TON	TONS	SY	SY	GAL	EA	EA
2022CPT.07.15.20411	Guilford	32	SR 3093 - OLD JULIAN RD	FROM NC 62 TO SR 3336 - COBLE CHURCH RD	1	2	2WU	NO	NO	3.594	21	200		377	3,471	233	50	45,675	15,073					
TOTAL FOR MAP NO. 32												200	377	3,471	233	50	45,675	15,073						
2022CPT.07.15.20411	Guilford	33	SR 3092 - WILD TURKEY RD	FROM SR 3093 - OLD JULIAN RD TO SR 3088 - THACKER DAIRY RD	1	2	2WU	NO	NO	2.027	21	75		354	1,937	130	6	25,483	8,410					
TOTAL FOR MAP NO. 33												75	354	1,937	130	6	25,483	8,410						
2022CPT.07.15.20411	Guilford	34	SR 3045 - MT HOPE CHURCH RD	FROM SR 3111 - HOLTS STORE RD TO SR 3114 - MCPHERSON-CLAY RD	1	2	2WU	NO	NO	1.648	21	75		346	1,520	102	566	20,000	6,600					
TOTAL FOR MAP NO. 34												75	346	1,520	102	566	20,000	6,600						
2022CPT.07.15.20411	Guilford	35	SR 3103 - FOSTER RD	FROM SR 3045 - MT HOPE CHURCH RD TO NC 61	1	2	2WU	NO	NO	1.709	21	105		342	1,562	105	235	20,557	6,784					
TOTAL FOR MAP NO. 35												105	342	1,562	105	235	20,557	6,784						
2022CPT.07.15.20411	Guilford	36	SR 2970 - JOHN WASHINGTON RD	FROM SR 2730 - JACKSON SCHOOL RD TO DEAD END	1	2	2WU	NO	NO	0.549	20.5	70		175	513	34	20	6,740	2,224					
TOTAL FOR MAP NO. 36												70	175	513	34	20	6,740	2,224						
2022CPT.07.15.20411	Guilford	37	SR 3144 - GUSENBURY RD	FROM NC 61 TO DEAD END	1	2	2WU	NO	NO	0.318	19	28		167	276	18	113	3,630	1,198					
TOTAL FOR MAP NO. 37												28	167	276	18	113	3,630	1,198						
2022CPT.07.15.20411	Guilford	38	SR 4760 - GOLF HOUSE ROAD WEST	FROM US 70 TO SR 4700 - GOLF HOUSE ROAD EAST	2	2	2WU	NO	NO	0.973	32.5		20,773	592	1,452	97	24			17	10	300		
TOTAL FOR MAP NO. 38													20,773	592	1,452	97	24		17	10	300			
2022CPT.07.15.20411	Guilford	39	SR 4700 - GOLF HOUSE ROAD EAST	FROM US 70 TO SR 4760 - GOLF HOUSE ROAD WEST	2	2	2WU	NO	NO	0.995	1		19,309		1,332	89	5			15	20			
TOTAL FOR MAP NO. 39													19,309		1,332	89	5		15	20				
2022CPT.07.15.20411	Guilford	40	SR 4775 - CHESNEY WAY	FROM SR 4760 - GOLF HOUSE ROAD WEST TO CUL-DE-SAC	2	2	2WU	NO	NO	0.115	26.5		2,446		168	11				3	3			
TOTAL FOR MAP NO. 40													2,446		168	11			3	3				
2022CPT.07.15.20411	Guilford	41	SR 4776 - CROSWELL CT	FROM SR 4775 - CHESNEY WAY TO CUL-DE-SAC	2	2	2WU	NO	NO	0.135	22.5		2,323		160	11				3	1			
TOTAL FOR MAP NO. 41													2,323		160	11			3	1				
2022CPT.07.15.20411	Guilford	42	SR 2807 - GANTWOOD LN	FROM SR 4760 - GOLF HOUSE ROAD WEST TO END MAINT	2	2	2WU	NO	NO	0.28	26.5	3	4,335		299	20				4	5			
TOTAL FOR MAP NO. 42												3	4,335		299	20			4	5				
2022CPT.07.15.20411	Guilford	43	SR 4711 - CLUBSIDE DR	FROM SR 4704 - GREY ROCK RD TO CUL-DE-SAC	2	2	2WU	NO	NO	0.459	26.5		8,657		597	40				10	11			
TOTAL FOR MAP NO. 43													8,657		597	40			10	11				
2022CPT.07.15.20411	Guilford	44	SR 4750 - CLUBSIDE CT	FROM SR 4711 - CLUBSIDE DR TO CUL-DE-SAC	2	2	2WU	NO	NO	0.026	23		852		59	4				1				
TOTAL FOR MAP NO. 44													852		59	4			1					
2022CPT.07.15.20411	Guilford	45	SR 4712 - WOOLWINE CT	FROM SR 4711 - CLUBSIDE DR TO CUL-DE-SAC	2	2	2WU	NO	NO	0.067	22		1,392		96	6				2	1			
TOTAL FOR MAP NO. 45													1,392		96	6			2	1				
2022CPT.07.15.20411	Guilford	46	SR 4747 - TIGARD CT	FROM SR 4711 - CLUBSIDE DR TO CUL-DE-SAC	2	2	2WU	NO	NO	0.036	22.5		1,003		69	5				1				
TOTAL FOR MAP NO. 46													1,003		69	5			1					
2022CPT.07.15.20411	Guilford	47	SR 4748 - MCKENZIE CT	FROM SR 4711 - CLUBSIDE DR TO CUL-DE-SAC	2	2	2WU	NO	NO	0.023	47.16		821		57	4				1				
TOTAL FOR MAP NO. 47													821		57	4			1					
2022CPT.07.15.20411	Guilford	48	SR 4704 - GREY ROCK RD	FROM SR 4700 - GOLF HOUSE ROAD EAST TO CUL-DE-SAC	2	2	2WU	NO	NO	0.389	26.5		6,828		481	32				9	11			
TOTAL FOR MAP NO. 48													6,828		481	32			9	11				
2022CPT.07.15.20411	Guilford	49	SR 4705 - BROOKSTONE DR	FROM SR 4700 - GOLF HOUSE ROAD EAST TO SR 4704 - GREY ROCK RD	2	2	2WU	NO	NO	0.202	26.5		3,293		227	15				4	2			
TOTAL FOR MAP NO. 49													3,293		227	15			4	2				
2022CPT.07.15.20411	Guilford	50	SR 4706 - BARWICK CT	FROM SR 4704 - GREY ROCK RD TO CUL-DE-SAC	2	2	2WU	NO	NO	0.082	22.5		1,610		111	7				2	1			
TOTAL FOR MAP NO. 50													1,610		111	7			2	1				
2022CPT.07.15.20411	Guilford	51	SR 4761 - THURLOW CT	FROM SR 4704 - GREY ROCK RD TO CUL-DE-SAC	2	2	2WU	NO	NO	0.032	39		944		65	4				1	1			
TOTAL FOR MAP NO. 51													944		65	4			1	1				
2022CPT.07.15.20411	Guilford	52	SR 4730 - GLENDEVON CT N	FROM SR 4728 - GLENDEVON DR TO CUL-DE-SAC	2	2	2WU	NO	NO	0.087	22.5		1,651		114	8				2	1			
TOTAL FOR MAP NO. 52													1,651		114	8			2	1				
2022CPT.07.15.20411	Guilford	53	SR 4729 - GLENDEVON CT S	FROM SR 4728 - GLENDEVON DR TO CUL-DE-SAC	2	2	2WU	NO	NO	0.097	22.5		1,963		135	9				2	4			
TOTAL FOR MAP NO. 53													1,963		135	9			2	4				
2022CPT.07.15.20411	Guilford	54	SR 4728 - GLENDEVON DR	FROM SR 4700 - GOLF HOUSE ROAD EAST TO SR 4730 - GLENDEVON CT N	2	2	2WU	NO	NO	0.124	26.5		2,077		143	10				2	1			
TOTAL FOR MAP NO. 54													2,077		143	10			2	1				
2022CPT.07.15.20411	Guilford	55	SR 4701 - HADDINGTON RD	FROM SR 4700 - GOLF HOUSE ROAD EAST TO SR 4702 - HADDINGTON COURT NORTH	2	2	2WU	NO	NO	0.059	1		1,120		77	5								
TOTAL FOR MAP NO. 55													1,120		77	5								
2022CPT.07.15.20411	Guilford	56	SR 4702 - HADDINGTON COURT NORTH	FROM SR 4701 - HADDINGTON RD TO CUL-DE-SAC	2	2	2WU	NO	NO	0.094	26.5		1,895		131	9				2	2			
TOTAL FOR MAP NO. 56													1,895		131	9			2	2				
2022CPT.07.15.20411	Guilford	57	SR 4703 - HADDINGTON COURT SOUTH	FROM SR 4701 - HADDINGTON RD TO CUL-DE-SAC	2	2	2WU	NO	NO	0.246	26.5		4,144		286	19				6	4			
TOTAL FOR MAP NO. 57													4,144		286	19			6	4				
2022CPT.07.15.20411	Guilford	58	SR 4769 - WYCKSHIRE CT	FROM SR 4700 - GOLF HOUSE ROAD EAST TO CUL-DE-SAC	2	2	2WU	NO	NO	0.198	26.5		3,530		244	16				3	3			
TOTAL FOR MAP NO. 58													3,530		244	16			3	3				
2022CPT.07.15.20411	Guilford	59	SR 4770 - NORTHWYCK DR	FROM SR 4769 - WYCKSHIRE CT TO SR 4769 - WYCKSHIRE CT	2	2	2WU	NO	NO	0.129	26.5		2,002		138	9				3	3			
TOTAL FOR MAP NO. 59													2,002		138	9			3	3				
TOTAL FOR PROJ NO. 2022CPT.07.15.20411												753	5,566	26,333	1,764	1,174	228,406	7,826	78,348	134	122	300		
GRAND TOTAL												23.778	753	117,758	5,566	26,333	1,764	1,174	228,406	7,826	78,348	134	122	300

